

A G E N D A
CANBY PLANNING COMMISSION
REGULAR MEETING
CITY COUNCIL CHAMBERS
MONDAY, JUNE 26, 1995
7:30 P.M.

I. ROLL CALL

II. MINUTES

May 22, 1995
June 5, 1995

III. CITIZEN INPUT ON NON-AGENDA ITEMS

IV. COMMUNICATIONS

V. NEW BUSINESS

VI. FINDINGS

SUB 95-03- Deuce Development/Dyches

VII. PUBLIC HEARINGS

DR 95-09, an application by Mark Crorey [applicant] and Michael Vigus [owner] for approval to construct a 2,492 square foot building to be used for either retail commercial use or office use. The property is located on the northwest corner of S. Ivy and S.W. 2nd Avenue [Tax Lot 8000 of Tax Map 3-1E-33CD]. *Continued from June 12, 1995*

SUB 95-04, an application by Regan Enterprises for approval to develop Phase 8 of Township Village, a 97-lot subdivision. The site is located south of S.E. 7th Avenue, west of the western ends of S.E. 8th, 9th, and 10th Avenues, and north of the Philander Lee and Ackerman Junior High campuses [Tax Lots 4800 of Tax Map 4-1E-4AA and Tax Lot 700 of Tax Map 4-1E-4A].

DR 95-11, an application by Peck Smiley Ettlín Architects [applicant] and Canby Fire District No. 62 [owner] for approval to construct a 14,215 square foot 1-story masonry fire station with double loaded 5-bay apparatus bay, administrative offices, living quarters, and meeting space. The site is located on the west side of S. Pine Street, between S.E. 2nd Avenue and S.E. 3rd Avenue [Tax Lot 804 of Tax Map 3-1E-33DA].

VIII. DIRECTOR'S REPORT

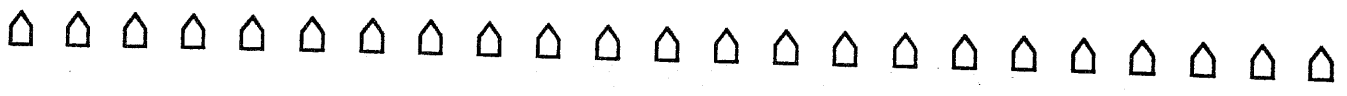
IX. ADJOURNMENT

THE CITY OF CANBY PLANNING COMMISSION WELCOMES YOUR INTEREST IN THESE AGENDA ITEMS. PLEASE FEEL FREE TO COME AND GO AS YOU PLEASE.

KURT SCHRADER, CHAIR
DAN EWERT

LINDA MIHATA, VICE-CHAIR
TAMARA MAHER

BOB GUSTAFSON
CARLIN JACKSON



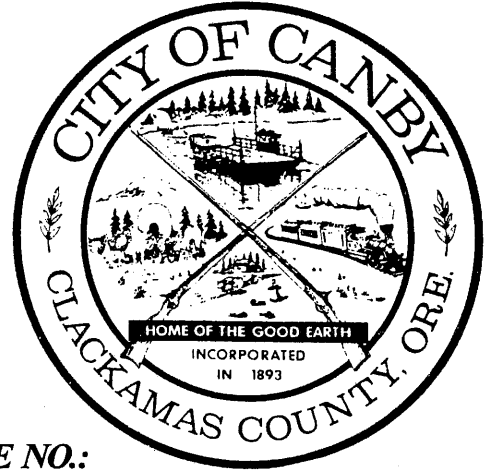
MEETING TIMELINES AND PROCEDURES

- In order not to restrict any person from testifying but, rather, to encourage everyone to do so, the Canby Planning Commission shall try to adhere as closely as possible to the following timelines:
 - Applicant (or representative[s]) - not more than 15 minutes
 - Proponents - not more than 5 minutes
 - Opponents - not more than 5 minutes
 - Rebuttal - not more than 10 minutes
- Everyone present is encouraged to testify, even if it is only to concur with previous testimony. For more complete presentations, Proponents and Opponents may "buy" time from one another. In so doing, those either in favor, or opposed, may allocate their time to a spokesperson who can represent the entire group.
- All questions must be directed through the Chair.
- Any evidence to be considered must be submitted to the hearing body for public access.
- All written testimony received, both for and against, shall be summarized by staff and presented briefly to the hearing body during presentation of the Staff Report.

The applicable substantive criteria for evaluating the application are displayed on the walls. Please direct your testimony to these criteria or other criteria in the Plan or land use regulations which you believe apply to the decision. Failure to raise an issue at this hearing with sufficient specificity to afford the Commission or Council and the parties an opportunity to respond to the issue precludes appeal to LUBA on that issue. A decision shall be made by the hearing body at the close of the hearing or the matter will be continued to a date certain in the future. This will be the only notice of that date that you will receive.



- STAFF REPORT -



APPLICANT:

Mark Crorey
925 N.E. 34th Place
Canby, OR 97013

FILE NO.:

DR 95-09

OWNER:

Michael Vigus
551 S. Holly Street
Canby, OR 97013

STAFF:

James S. Wheeler
Assistant Planner

LEGAL DESCRIPTION:

Tax Lot 8000 of Tax Map 3-1E-33CD

DATE OF REPORT:

June 16, 1995

LOCATION:

Northwest corner of S.W. 2nd Avenue
and S. Ivy Street

DATE OF HEARING:

June 26, 1995
(Continued from June 12, 1995)

COMP. PLAN DESIGNATION:

Highway Commercial

ZONING DESIGNATION:

C-2 (Highway Commercial)

I. APPLICANT'S REQUEST:

The applicant is requesting site and design approval to construct a 2,400 square foot office or retail commercial building.

II. APPLICABLE REGULATIONS

- **City of Canby General Ordinances:**
 - 16.10 Off-Street Parking and Loading
 - 16.28 C-2 - Highway Commercial Zone
 - 16.49 Site and Design Review
 - 16.88 General Standards

III. MAJOR APPROVAL CRITERIA

Site and Design Review

The Planning Commission, sitting as the Design Review Board, shall, in exercising or performing its powers, duties or functions, determine whether there is compliance with the following:

- A. The proposed site development, including the site plan, architecture, landscaping and graphic design, is in conformance with the standards of this and other applicable City ordinances insofar as the location, height and appearance of the proposed development are involved; and
- B. The proposed design of the development is compatible with the design of other developments in the same general vicinity; and
- C. The location, design, size, color and materials of the exterior of all structures and signs are compatible with the proposed development and appropriate to the design character of other structures in the same vicinity.

The Design Review Board shall, in making its determination of compliance with the requirements set forth, consider the effect of its action on the availability and cost of needed housing.

IV. FINDINGS:

A. Background and Relationships:

The applicant is requesting approval to construct a new, 2,400 square foot commercial building. The building is to be used for retail commercial or offices.

The roof on the porch of the home on the property immediately to the west encroaches on this property by approximately 1.5 feet. The site plan shows the

proposed building to be located approximately 1.5 feet from the western property line to accommodate the encroachment.

The application was originally submitted with access from S. Ivy Street. The responses to the "Request For Comments" brought serious concerns regarding access to this property from S. Ivy Street. The concern were specifically related to the fact that 2nd Avenue is offset, with the west portion being located immediately to the north of this property, and the east portion being located a little bit south of this property, on the other side of S. Ivy Street. And secondly, this stretch of S. Ivy Street is the busiest street in Canby, not including Highway 99-E. The applicant decided to attempt a redesign of the site to have the access from 2nd Avenue instead of S. Ivy Street. Due to the site constrictions, staff believes that the site design submitted is virtually the only workable site layout, with access from 2nd Avenue and a 2400 square foot general commercial building.

B. Evaluation Regarding Site and Design Review Approval Criteria

- 1. Part IV - Section 2, No. 2**
"Minimum area for landscaping is 15% of the total area to be developed."

The overall site has been developed. The minimum amount of landscaping required for the 8,000 square feet lot is 1,200 square feet (15%). The total amount of landscaping proposed for the lot is 2,216 square feet (27.7%).

- 2. Parking.**

There is no specific use proposed for the building. The number of parking spaces proposed, twelve (12), is sufficient to provide enough parking for all commercial uses except restaurants. Use of a small portion of the building for a restaurant (ice cream vendor, for example) is theoretically possible, provided that the parking demand generated by the use in the remaining portion of the building is small enough to compensate for the higher parking demand of the restaurant use. The parking ratio that is proposed is 5.0 spaces per 1,000 square feet of building space.

One "van accessible" handicap parking space is required. The northernmost parking space on the western side of the parking lot will be a handicap parking space. There will be two compact parking spaces. The northernmost parking space on the eastern side of the parking lot will need to have an area available for backing and turning. The walkway shown in front of the building, on the 2nd Avenue side, is longer than what is required for the connection from the doorway to the

Ivy Street sidewalk. If the sidewalk is shortened up, there will be sufficient room for the backing/turning movement for the northeastern parking space. Wheel stops will be required for all parking spaces, and the compact parking spaces need to be so designated.

No loading facilities are required.

3. Access

Access to the site is from S.W. 2nd Avenue. The guy wire for the existing electric pole is in the location of the driveway. It is proposed that either the electric pole will be moved to the east, beyond the driveway, or a second electric pole will be placed east of the driveway, and the guy wire for the existing electric pole will be removed. The driveway is at least fifty (50) feet from the intersection of S.W. 2nd Avenue and S. Ivy Street. The driveway is shown to be fifteen (15) feet wide at the curb of S.W. 2nd Avenue. The driveway needs to be at least twenty (20) feet wide in order to provide two-way access at the entrance. This will mean that the western edge of the driveway will be nineteen (19) feet from the western property line, and more importantly, one (1) foot from the back edge of the western parking spaces. This is tight, but will be workable, and is the only solution for access from S.W. 2nd Avenue.

Improvements to S. Ivy Street will be necessary. No additional right-of-way dedication along S. Ivy Street (other than possible curb return dedication at the intersection of 2nd Ave. and Ivy St.) will be needed, as the current right-of-way width of sixty (60) feet meets the minimum standards for arterial right-of-way widths as provided in the City's adopted Transportation Master Plan. Needed improvements along S. Ivy include curbs, sidewalks, some street widening, and storm drainage. The improvements need to meet the County's as well as the City's standards for construction, as S. Ivy is a County road. A County "Street Construction and/or Encroachment Permit" will be required by the County for the improvements on S. Ivy Street.

4. Architecture

The building will have a combination of brick, synthetic stucco, and lap siding. The roof will have "cedar shake look" asphalt shingles. There will be a sixteen (16) foot high concrete brick wall that will serve as a parapet wall on the west side of the building, next to the house, and for the rear wall of the building. The roof line on the west side of the building, next to the existing home, will be fourteen (14) feet high, two

feet lower than the height of the parapet wall. At the east end of the building, toward S. Ivy Street, the height will be twenty (20) feet.

No signage is proposed to be on the building itself. A pole sign, the bottom of which will be fifteen (15) feet off of the ground, will be along S. Ivy Street. The amount of signage is proposed to be less than eighty (80) square feet on either side of the two-sided pole sign, thus the total amount of signage will be less than 160 square feet, the maximum permitted for this size of building in this zone. There is no specific designs for the sign as the building is to be rented out and no tenant has been designated. All parts of the sign must be located no closer than ten (10) feet from the face of the curb, or edge of the pavement, along S. Ivy Street and must be fully within the property line.

5. Other Aspects

a. Utilities

Service providers have not indicated that there would be any problem in servicing this proposal. An existing gas service line will need to be abandoned.

b. Landscaping

The landscape area is between the building and S. Ivy Street. The applicant is proposing to retain the two locust trees, while removing the oak and holly trees. The remaining landscaping is proposed to be lawn.

c. Parking Lot Landscaping

There are to be 12 parking spaces and approximately 3100 square feet of paving area. Approximately 520 square feet of landscaping will be provided within 10 feet of the approximately 3100 square feet paved area (16.8%). There is 1 tree, the large multi-trunk locust tree, within 10 feet of the paved vehicle parking/maneuvering area.

d. Density and yards and height

The setbacks and the height requirements for the C-2 zone have been met by this development proposal.

V. CONCLUSION

The staff hereby concludes that, with appropriate conditions, the proposed development as described in the application, site plan, and this report, is in conformance with the standards of this and other applicable ordinances; the design is compatible with the design of other developments in the vicinity; and, the location, design, size, and materials of the exterior of the structure will be compatible with the proposed development and appropriate to the design character of other structures in the same vicinity.

Further, staff concludes that, with approval conditions:

1. the proposed development of the site is consistent with the applicable standards and requirements of the Canby Municipal Code and other applicable City ordinances insofar as the location, height and appearance of the proposed development are involved; and
2. the proposed design for the development is compatible with the design of other developments in the same general vicinity; and
3. the location, design, size, color and materials of the exterior of all structures and signs are compatible with the proposed development and appropriate to the design character of other structures in the same vicinity; and
4. the conditions listed are the minimum necessary to achieve the purposes of the Site and Design Review Ordinance, and do not unduly increase the cost of housing.

VI. RECOMMENDATION:

Based upon the application, elevations, the site plan received by the City, the facts, findings and conclusions of this report, and without the benefit of a public hearing, staff recommends that should the Planning Commission approve DR 95-09, the following conditions apply:

For the Building Permit Application:

1. A detailed landscape plan shall be submitted with the building permit. The detailed landscape plan shall show: the number of plants, plant spacing/location of planting, the type of plants, the size of plants, the schedule of planting, and irrigation plans.
2. Automatic irrigation shall be provided for the lawn landscaping.
3. The walkway in front (on the south-facing side) of the building shall terminate at the door to the building, allowing for a minimum of two (2) feet of backing/turning movement for the northeastern parking space.

4. The driveway shall be twenty (20) feet wide. The eastern edge of the driveway shall be no closer than fifty (50) feet from the curb intersection of S.W. 2nd Avenue and S. Ivy Street, and the western edge of the driveway shall be no closer than eighteen (18) feet from the western property line.
5. All parking spaces shall have wheel stops.
6. The handicap parking space and the compact parking spaces shall be designated as such.

Prior to Construction:

7. The site is to have the address posted on the property prior to construction, for emergency service identification.

Prior to Occupancy:

8. S. Ivy Street shall be improved with curbs, sidewalks, street widening, and storm sewers. The improvements shall meet the standards of the County and the City. A "Street Construction and/or Encroachment Permit" shall be obtained from the County.
9. The electric pole on S.W. 2nd Avenue shall be relocated, or another pole constructed such that there will be no obstruction of the driveway entrance from any utility pole or guy wire.
10. A "van accessible" parking space shall be provided. It shall meet the American Disabilities Act requirements regarding access to the building.
11. The sidewalks shall be located against the curb, and shall be five-feet wide, including the curb. Where mailboxes, newspaper boxes or other obstructions (such as fire hydrants) are located at the curb, the sidewalk shall be set away from the curb such that the sidewalk remains unobstructed for a full five-foot width.
12. The existing gas service shall be abandoned.
13. All of the sign shall be located no closer than ten (10) feet from the face of the curb or the edge of the pavement along S. Ivy Street, and shall be wholly within the property.

Notes:

14. If any restaurant use occupies the building, the use of the remaining portions of the building will be limited by the available number of parking spaces.

Exhibits:

1. Application for Design Review
2. Vicinity Map
3. Site Plan/Elevations/Landscape Plan
4. Department Responses to "Request for Comments"

SITE AND DESIGN REVIEW APPLICATION

Fee: \$750

OWNER

APPLICANT

Name VIGUS
Address 551 S. Holly
City Canby State OR Zip 97013
SIGNATURE see attached

Name MARK CROLEY
Address 925 N.E 34th PLACE
City CANBY State OR Zip 97013
Phone: 206-6156

DESCRIPTION OF PROPERTY:

Tax Map 3-1E-33CD Tax Lot(s) 8000 Lot Size 80'x100' = .18 AC
(Acres/Sq. Ft.)

or

Legal Description, Metes and Bounds (Attach Copy)

Plat Name _____ Lot _____ Block _____
NW corner 2nd & Hwy
DRH

PROPERTY OWNERSHIP LIST

Attach a list of the names and addresses of the owners of properties located within 200 feet of the subject property (if the address of the property owner is different from the situs, a label for the situs must also be prepared and addressed to "Occupant"). Lists of property owners may be obtained from any title insurance company or from the County Assessor. If the property ownership list is incomplete, this may be cause for postponing the hearing. The names and addresses are to be typed onto an 8-1/2 x 11 sheet of labels, just as you would address an envelope.

USE

Existing _____
Proposed _____

Existing Structures _____

Surrounding Uses Commercial

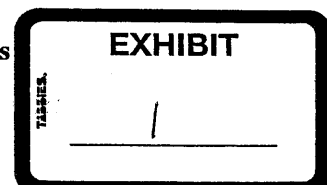
PROJECT DESCRIPTION Commercial / office Mixed use BLDG ~ 2600sq ft

ZONING Heavy Comm. COMPREHENSIVE PLAN DESIGNATION _____

PREVIOUS ACTION (if any) _____

File No. DR 95-09
Receipt No. 9236
Received by BARMS
Date Received 12-15-94 12/15/94
Completeness Date 5/12/95
Pre-Ap Meeting _____
Hearing Date 6-12-95

* If the applicant is not the property owner, he must attach documentary evidence of his act as agent in making application.



PROPOSED MULTI-USE NEW CONSTRUCTION 2ND & IVY

PROJECT OVERVIEW:

The proposed project is a 2400 sq. ft. commercial building. There are 12 parking spaces provided, including 1 handicapped space. The project is a 1 story construction with mechanical room in the attic space. The building is offset 1.5 ft from the NW corner of the lot so not as to interfere with the existing encroachment of the existing porch roof of the dilapidated structure to the west.

The structure will have a combination of brick, synthetic stucco, and lap sidings. The roof will be "cedar shake look" asphalt shingles, and, some flat roof on the rear of the building. There is a 16' high concrete block wall that serves as a parapet wall and the rear wall of the structure.

Automobile ingress and egress is from 2nd St.. A new 6' wide sidewalk along Ivy is to connect to the existing sidewalk on 2nd.

Landscaping would include leaving the existing Locust trees located towards the front of the lot, with the balance of the lot being landscaped in grass.

Tenancy is unknown at this time, however, parking is adequate for: medical office, commercial, or standard office uses.

A two sided sign set 15' off the ground on two posts is proposed. The total sq. footage of the sign is 160 SQ. FT. The sign will be set back from Ivy street 13.5'.

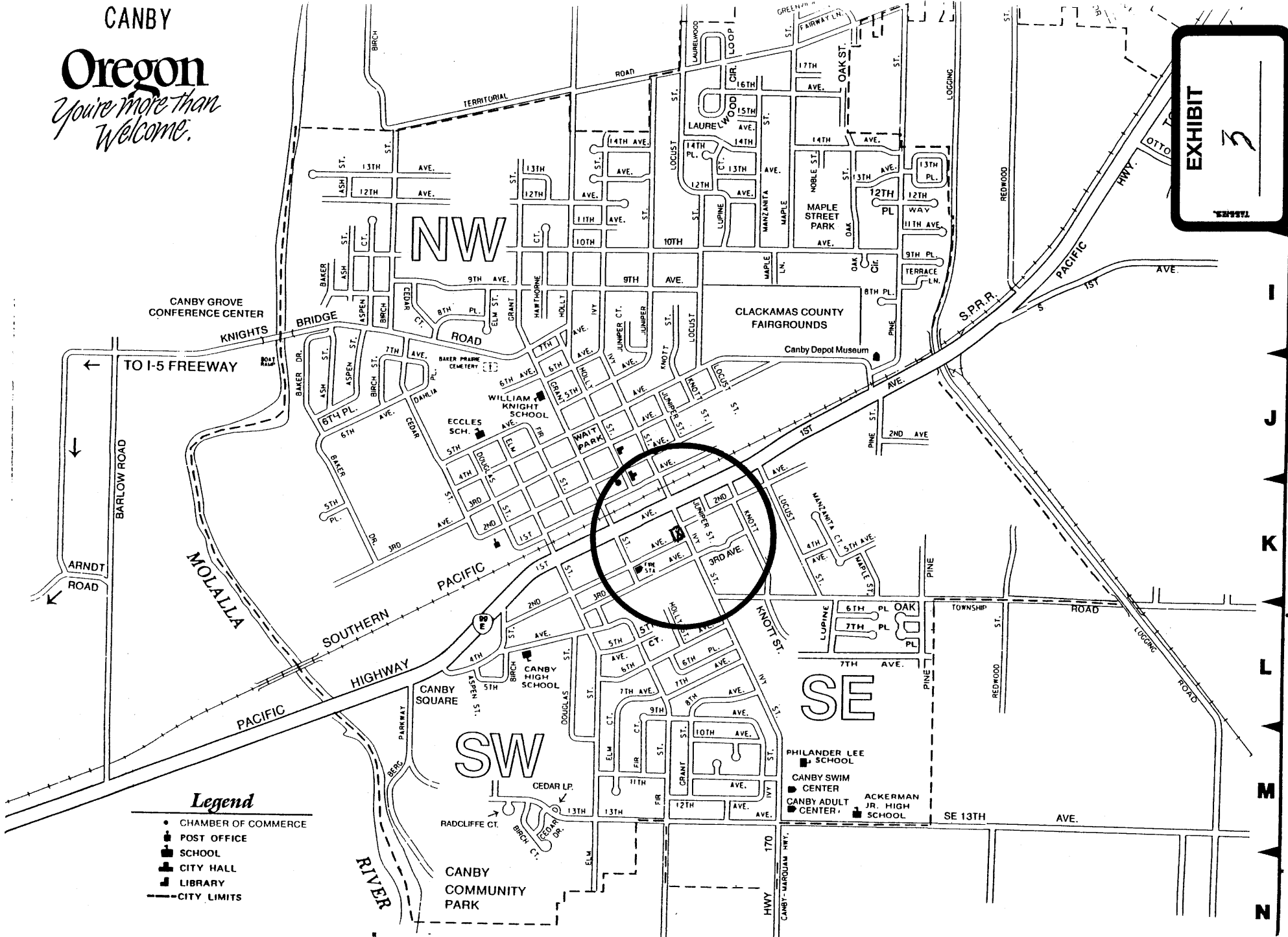
EXHIBIT

2

CANBY

Oregon
*You're more than
Welcome.*

EXHIBIT
3



Legend

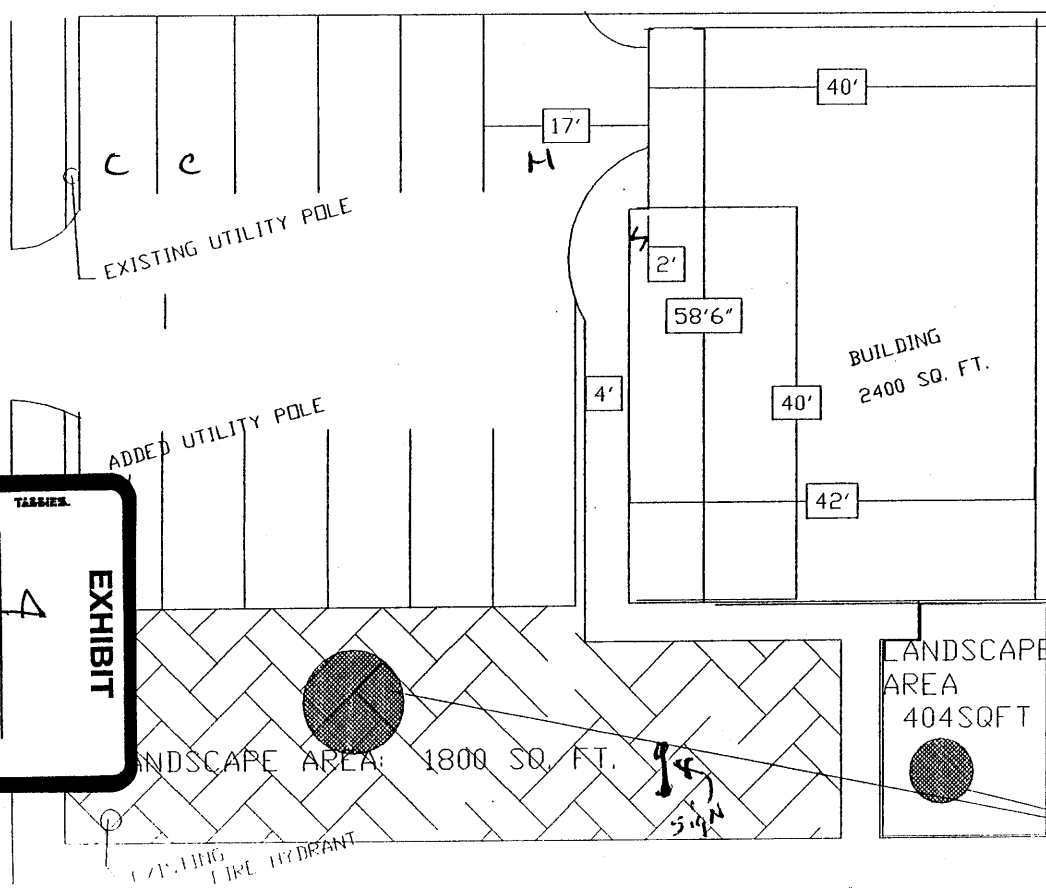
- CHAMBER OF COMMERCE
- POST OFFICE
- SCHOOL
- CITY HALL
- LIBRARY
- CITY LIMITS

2ND & IVY PLOT PLAN

SCALE: 1"=20'

NOTES:

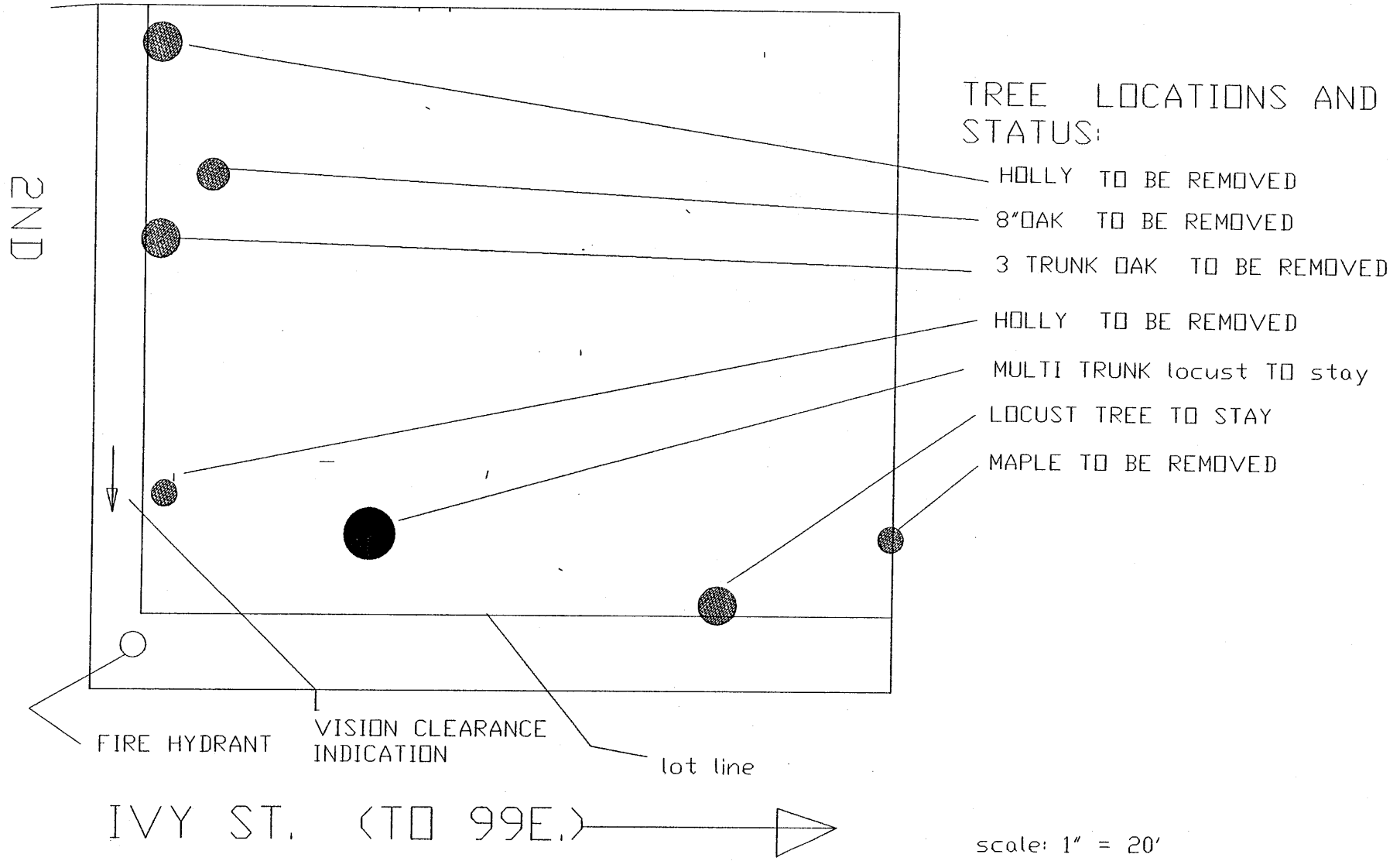
- BUILDING IS OFFSET FROM CORNER OF LOT BY 1.5'
- 12 PARKING SPACES INCLUDING 1 HANDICAPPED
- LANDSCAPE AREA > 2,200 SQ. FT.
- BUILDING IS 2,400 SQ. FT.
- UTILITY POLE SUPPORT WIRE TO BE REMOVED NEW SUPPORT POLE AND WIR TO BE ADDED



TERRACE
4
5 pages
EXHIBIT

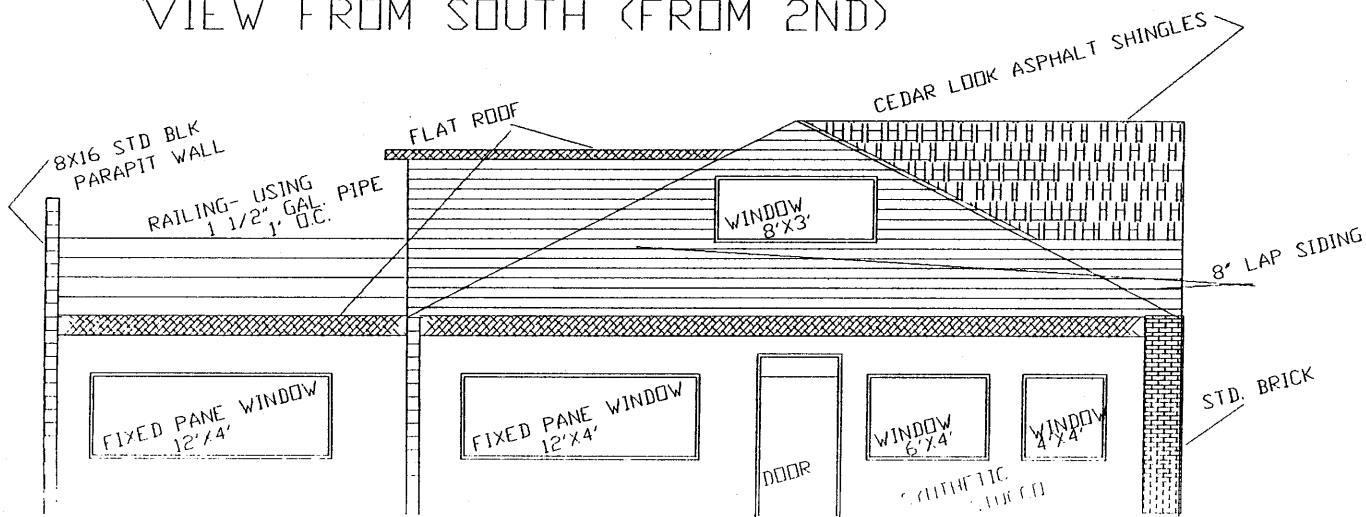
EXISTING PLANTINGS MAP

CORNER OF 2ND & IVY

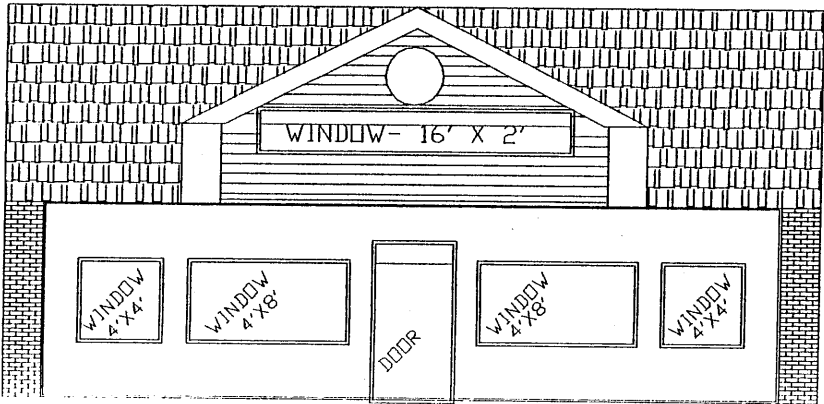


2ND & IVY ELEVATION VIEW
VIEW FROM SOUTH (FROM 2ND)

SCALE: 1"=10'



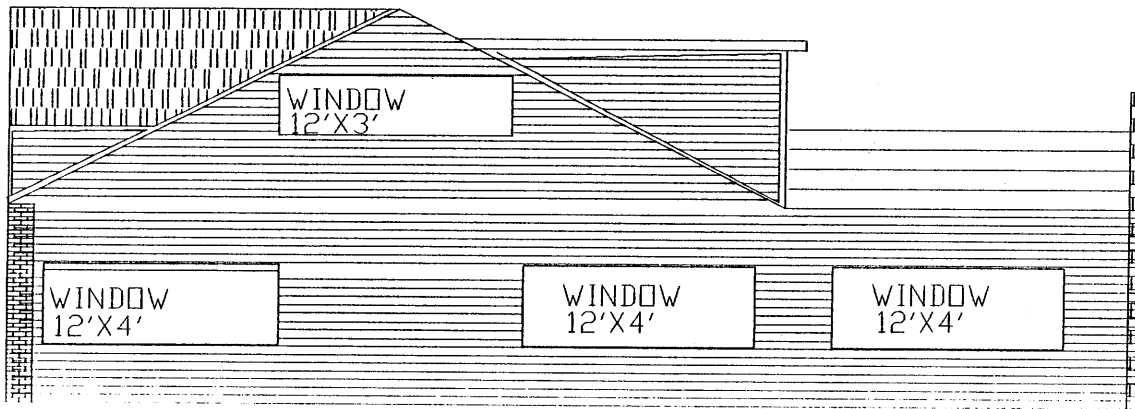
2ND & IVY FRONT ELEVATION VIEW



SCALE 1"

NORTH ELEVATION VIEW
2ND & IVY

SCALE 1"=10'



PLEASE RETURN ATTACHMENTS!!!

CANBY PLANNING DEPARTMENT REQUEST FOR COMMENTS

P.O. Box 930, Canby, OR 97013

[503] 266-4021

DATE: May 15, 1995

TO: FIRE, POLICE, CUB, TELEPHONE, TELECOM, NW NATURAL GAS, JOHN KELLEY,
CLACKAMAS COUNTY PLANNING [BILL SPEARS], ROY, STEVE, MIKE

The City has received DR 95-09, an application by Mark Crorey [applicant] and Michael Vigus [owner] for approval to construct a 2,492 square foot building to be used for either retail commercial use or office use. The property is located on the northwest corner of S. Ivy and S.W. 2nd Avenue [Tax Lot 8000 of Tax Map 3-1E-33CD].

We would appreciate your reviewing the enclosed application and returning your comments by **May 30, 1995 PLEASE**. The Planning Commission plans to consider this application on **June 12, 1995**. Please indicate any conditions of approval you may wish the Commission to consider if they approve the application. Thank you.

Comments or Proposed Conditions:

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: Tom Pearson

Date: _____

EXHIBIT
5
9 pages

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Comments or Proposed Conditions:

The construction of this building is OKAY.
As stated in this application all trees should be removed.
The traffic congestion on S. Ivy and with S.W. 2nd &
S.E. 2nd being off set, this area is too congested
for ingress & egress on S. Ivy. I recommend that
the parking and building reversed so ingress & egress
comes off S.W. 2nd. We have 2 driveway cuts existing
on S.W. 2nd for this property. 1 can be removed.

Please check one box:

- Adequate Public Services (of your agency) are available
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- Adequate public services are not available and will not become available

Signature: Roy A. Hunter

Date: 5-17-95

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Comments or Proposed Conditions:

maybe a congestion problem with both TRAFFIC
S.R FROM HIGHWAY AND TRAFFIC AT INTERSECTION
of S.W. 2ND AND S. IVY. AND THE DRIVEWAY

Would prefer right turn only when exiting the
DRIVEWAY onto S. IVY.

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: Jerry Vigus

Date: 5/16/95

PLEASE RETURN ATTACHMENTS!!!

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Comments or Proposed Conditions:

None

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: *Steve Pearson*

Date: *5/16/95*

PLEASE RETURN ATTACHMENTS!!!

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Comments or Proposed Conditions:

REQUEST PROPER ADDRESSING BE IN PLACE PRIOR TO CONSTRUCTION

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: Ron Yarbrough

Date: 5-18-95

PLEASE RETURN ATTACHMENTS!!!

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Comments or Proposed Conditions:

NW Natural Gas has an existing service at this address - (see attached plat) that will need to be abandoned prior to any work starting.

Please check one box:

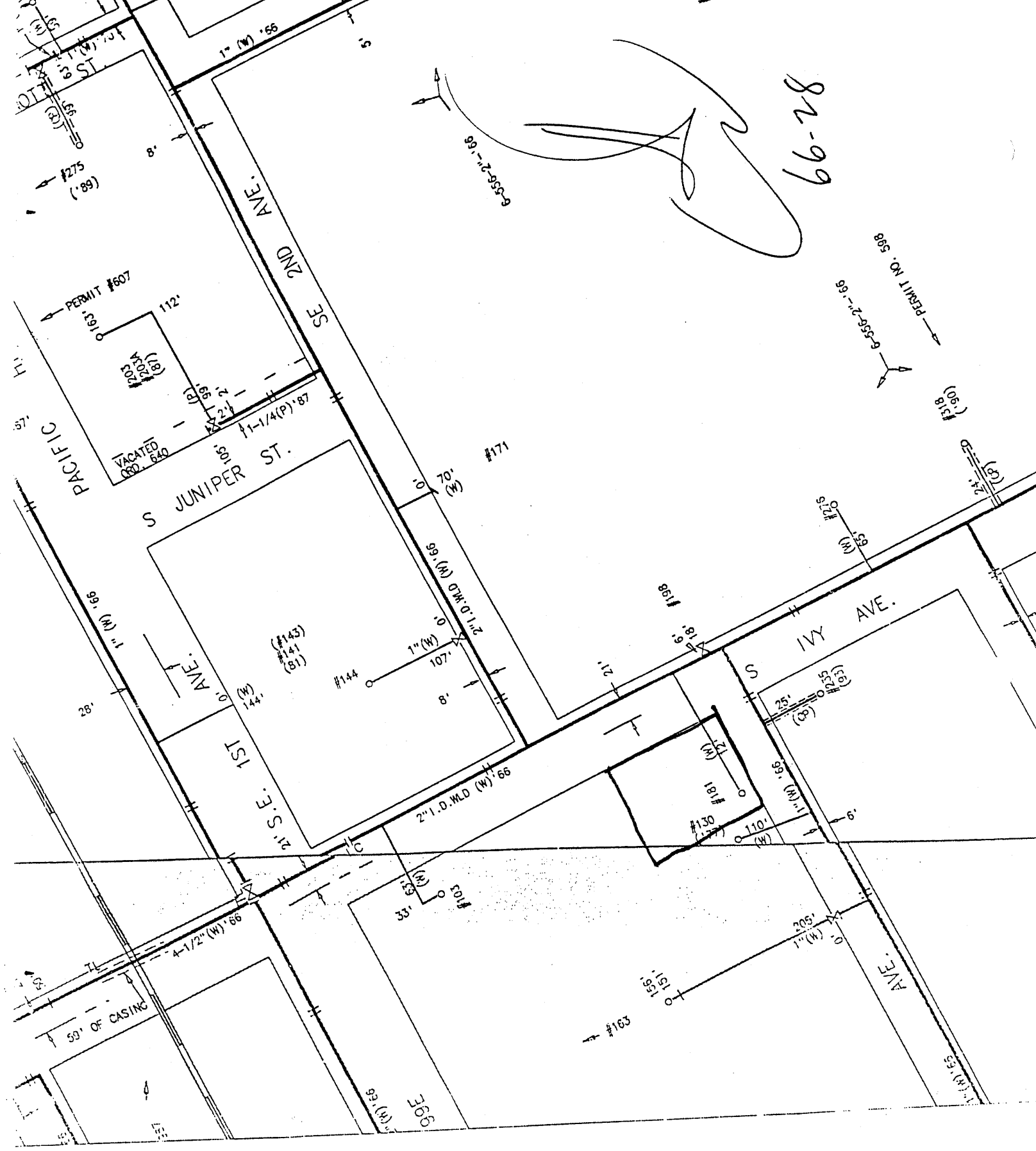
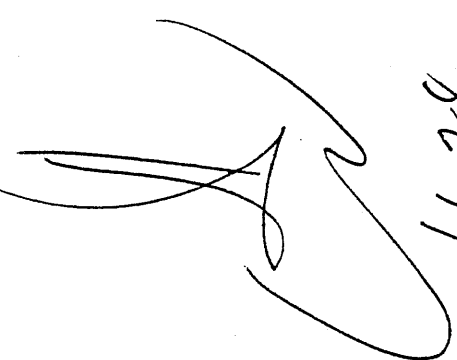
- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: _____

Gary Hyatt

Date: 5/19/95

82-99



PERMIT #607
#273 (68)

VACATED
#80, #84

#143
#141
#81

59' OF CASING

99' (W) '66

1" (W) '66

112'
1-1/4 (P) '87

1" (W) '66

4-1/2" (W) '66

99' (W) '66

99' (W) '66

107'
2" I.D. D.M. '66

2" I.D. M.D. (W) '66

33'
#103 (W) '63

99' (W) '66

99' (W) '66

107'
1" (W) '66

21'
2" I.D. M.D. (W) '66

151'
#163

99' (W) '66

99' (W) '66

110'
#130 (W) '66

25'
5.5" (S) '63

205'
1" (W) '61

99' (W) '66

PERMIT NO. 598
9-556-2'-66

PERMIT NO. 598
9-556-2'-66

118' (S) '63

118' (S) '63

99' (W) '66



CLACKAMAS COUNTY

Department of Transportation & Development

THOMAS J. VANDERZANDEN
DIRECTOR

MEMORANDUM

TO : CITY OF CANBY

FROM : CLACKAMAS COUNTY, DTD (BILL SPEARS)

DATE : MAY 26, 1995

RE : DR95-09 CROREY/VIGUS (EN95-304)
(SECOND AND IVY STREETS COMMERCIAL)

This office has the following comments pertaining to this proposal:

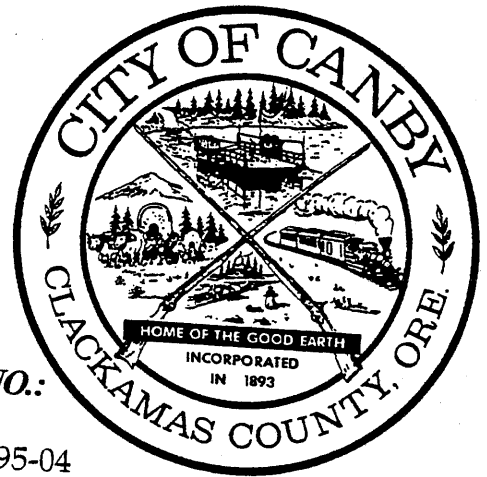
1. Ivy Street is a County Road and Second Street is a City Street. The County requests that the City of Canby require the developer to make up to a half street improvement on Ivy Street. The actual amount of work will depend upon an evaluation of the existing road surface. Repairs may be needed. At a minimum the applicant must construct the driveway entry to County standards and construct standard curb, surfacing, storm sewer facilities, sidewalk and pavement tapers. The curb shall align with the curb line established at Third Street and Ivy Street. The sidewalk shall be to City standards but shall be a minimum of five (5) feet in width plus the width of the curb. The sidewalk shall meet ADA requirements without obstructions (i.e., utility poles, pedestals and fire hydrants).
2. A right of way and curb radius shall be provided at the intersection of Ivy and Second Streets. This will probably require a road dedication. Clackamas County must be contacted.
3. Landscape materials cannot obscure site distance at the corner of Second and Ivy Streets nor at the driveway.
4. It appears as if there may be a utility pole and guy wire in the path of the sidewalk proposed on Second Street. Although this is a City issue it should probably be compensated for in the design.
5. Sufficient right of way must be dedicated from the subject property on Ivy Street to ultimately provide a seventy (70) foot right of way to accommodate a three lane facility with bike lanes, sidewalks and utilities.

6. Engineered plans, a Street Construction and/or Encroachment Permit, financial guarantee, inspection fee, and Street Opening Permit are required by Clackamas County for Ivy Street improvements. These plans must include surface water management and erosion control. All permits must be obtained prior to commencement of site work and building permit issuance.

BS/jb

<JeanB>BS/DR95-09/Crorey/Vigus

- STAFF REPORT -



APPLICANT:

Regan Enterprises
31233 French Prairie Rd.
Wilsonville, OR 97070

FILE NO.:

SUB 95-04
(Township Village Phase 8)

OWNER:

Regan Enterprises
31233 French Prairie Rd.
Wilsonville, OR 97070

STAFF:

James S. Wheeler
Assistant Planner

LEGAL DESCRIPTION:

Tax Lot 4800 of Tax Map 4-1E-4AA and
Tax Lot 700 of Tax Map 4-1E-4A

DATE OF REPORT:

June 16, 1995

LOCATION:

South of S.E. 7th Avenue, west of the
the western ends of S.E. 8th, 9th, and 10th
Avenues, and north of the Philander Lee
and Ackerman Junior High campuses

DATE OF HEARING:

June 26, 1995

COMP. PLAN DESIGNATION:

Low Density Residential

ZONING DESIGNATION:

R-1.5 (Medium Density Residential)

I. APPLICANT'S REQUEST:

The applicant is requesting approval for a ninety-seven (97) lot subdivision. The subdivision includes the extension of five (5) public roads, S. Larch Street, S. Lupine Street, S.E. 8th Avenue, S.E. 9th Avenue, and S.E. 10th Avenue.

II. CABLE CRITERIA:

City of Canby Code Section 16.62.020

This is a quasi-judicial land use application. Applications for a subdivision shall be evaluated based upon the following standards and criteria:

Conformance with the text and applicable maps of the Comprehensive Plan.

Conformance with other applicable requirements of the land development and planning ordinance.

The overall design and arrangement of lots shall be functional and shall adequately provide building sites, utility easements, and access facilities deemed necessary for the development of the subject property without unduly hindering the use or development of adjacent properties.

Other Applicable Policies and Regulations:

City of Canby General Ordinances:

- 6 R-1 Low Density Residential Zone
- 7 Major and Minor Partitions (Subdivisions)
(especially 16.64, Subdivision Design)
- 8 Street Alignment
- 9 General Standards

III. FT

A. Ground and Relationships

Subject parcel is located north of the Philander Lee and Ackerman Junior campuses, south of S.E. 7th Avenue, and west of S. Pine Street. The parcel is divided into 97 lots which will vary in size from 7,076 square feet to 14,474 square feet. The last phase of Township Village is the last phase. Originally, there were ten or more phases planned.

B. Comprehensive Plan Consistency Analysis

Citizen Involvement

GOAL: TO PROVIDE THE OPPORTUNITY FOR CITIZEN INVOLVEMENT THROUGHOUT THE PLANNING PROCESS

- Policy #1: Canby shall reorganize its citizen involvement functions to formally recognize the role of the Planning Commission in meeting the six required citizen involvement components of statewide planning goal No. 1, and to re-emphasize the City's commitment to ongoing citizen involvement.
- Policy #2: Canby shall strive to eliminate unnecessarily costly, confusing, and time-consuming practices in the development review process.
- Policy #3: Canby shall review the contents of the Comprehensive Plan every two years and shall update the Plan, as necessary, based upon that review.

ANALYSIS

1. The notification process and public hearing are a part of the compliance with adopted policies and process regarding citizen involvement. The Planning Commission seeks input of all citizens at the public hearing of all applications.
2. The Planning Commission adheres to acting upon applications within a sixty (60) day time period from the date of determination of a complete application. Any continuation of the review period is done with the approval of the applicant, or through admission of new information into the review process.
3. The review of the contents of the Comprehensive Plan is not germane to this application.

ii. Urban Growth

- **GOALS:**
 - 1) **TO PRESERVE AND MAINTAIN DESIGNATED AGRICULTURAL AND FOREST LANDS BY PROTECTING THEM FROM URBANIZATION.**
 - 2) **TO PROVIDE ADEQUATE URBANIZABLE AREA FOR THE GROWTH OF THE CITY, WITHIN THE FRAMEWORK OF AN EFFICIENT SYSTEM FOR THE TRANSITION FROM RURAL TO URBAN LAND USE.**

- Policy #1: Canby shall coordinate its growth and development plans with Clackamas County.

Policy #2: Canby shall provide the opportunity for amendments to the Urban Growth Boundary (subject to the requirements of statewide planning goal 14) where warranted by unforeseen changes in circumstances.

Policy #3: Canby shall discourage the urban development of properties until they have been annexed to the City and provided with all necessary urban services.

ANALYSIS

1. The property is entirely within both the Urban Growth Boundary and the City Limits. The project does not directly impact any County road.
2. No changes to the Urban Growth Boundary are proposed with this application. The property has been used agriculturally, for the growing of Christmas trees, however, it is fully within the current Urban Growth Boundary and has been acknowledged for 'future' urbanization. The applicant is requesting that the 'future' be considered now, as the property is within the City limits.
3. All necessary urban services are, or will be available for the subdivision (see discussion under Public Services Element).

iii. Land Use Element

■ **GOAL: TO GUIDE THE DEVELOPMENT AND USES OF LAND SO THAT THEY ARE ORDERLY, EFFICIENT, AESTHETICALLY PLEASING AND SUITABLY RELATED TO ONE ANOTHER.**

Policy #1 Canby shall guide the course of growth and development so as to separate conflicting or incompatible uses, while grouping compatible uses.

Policy #2 Canby shall encourage a general increase in the intensity and density of permitted development as a means of minimizing urban sprawl.

Policy #3 Canby shall discourage any development which will result in overburdening any of the community's public facilities or services.

Policy #4: Canby shall limit development in areas identified as having an unacceptable level of risk because of natural hazards.

Policy #5 Canby shall utilize the land use map as the basis of zoning and other planning or public facility decisions.

Policy #6: Canby shall recognize the unique character of certain areas and will utilize the following special requirements, in conjunction with the requirements of the Land Development and Planning Ordinance, in guiding the use and development of these unique areas.

Area "I" is a large area of a single ownership which includes property both inside and outside the City limits. It is presently in agricultural use with only one single-family dwelling. Planned eventual development of the property includes both medium and high density designations. Immediate development of the site is prevented because of the lack of public sewer service to the property and the need for significant improvements to S.E. Township Road between this property and S. Ivy Street. The zoning of Area "I" should be changed to R-1.5 and R-2 when sewer service is available and road improvements made. No additional residential development, beyond the one single-family dwelling per lot, will be allowed to occur without upzoning of the property. Annexation of the easterly portion of this area will be subject to the same requirements and will result in automatic upzoning. It should be noted that Area "I" was originally designated for primarily low density residential development. Its proposed density was increased partly to assist the developer in providing affordable housing in spite of the costs of physical improvements which will be needed.

ANALYSIS

1. The parcel is zoned R-1, Low Density Residential and is proposed to be developed with single family residential structures. The Comprehensive Plan land use designation of the subject parcel is Medium Density Residential, which is a higher designation than the current zoning and the proposed development. The current use of the property is for Christmas tree farming. The majority of the trees on the site have been harvested, the remaining trees are expected to be harvested prior to construction. The property is part of the Township Village master plan for residential development of the property.

The proposed development is compatible with the majority of the surrounding land uses. Immediately to the west is a mix of commercial uses that front on S. Ivy Street. To the south is the Philander Lee school

and the open space/play areas of the Philander Lee and Ackerman schools. To the north and east are located the previous phases of the Township Village subdivision development.

The proposed development density for the subject parcel (5.7 lots per developable acre) is higher than that of the Comprehensive Plan "density assumption" used to estimate the amount of area that is needed for low density residential areas (4.7 lots per developable acre, p.36). However, this density is substantially lower than that of the Comprehensive Plan "density assumption" used to estimate the amount of area that is needed for medium density residential areas (8.0 units per developable acre, p.36). The "density assumptions" are *averages* with some areas developing above the "average" and other areas developing below the "average".

The project density [5.7 lots per developable acre] is comparable to those of the surrounding subdivision developments: Township Village 5 [5.1 lots per developable acre], Township Village 6 [6.1 lots per developable acre], Township Village 7 [5.4 lots per developable acre], Valley Farms I [5.5 lots per developable acre], Valley Farms II [5.8 lots per developable acre], Valley Farms III [5.2 lots per developable acre]. Developable acres does not include streets. Overall, Township Village, including proposed phase 8, has been developed at the density of 5.7 units per developable acre.

2. The zoning of the property is R-1, Low Density Residential. The Comprehensive Plan land use designation of the property is Medium Density Residential, equivalent to R-1.5. The density of the subdivision is higher than what the Comprehensive Plan calculates for the amount of land needed for low residential development [4.7 lots per developable acre], but is lower than what the Comprehensive Plan calculated for the amount of land needed for medium residential development [8.0 units per developable acre]. The single-family residential developments that have occurred in medium- and high-density residential zones (Rebecca Estates [R-1.5], Morse Additions 1 and 2 [R-1.5], Township Village 1, 2, 4, 5, 6 [R-1.5], Township Village 3 [R-1.5/R-2]) have lowered the overall residential density of development below what the Comprehensive Plan has used to calculate the amount of land needed. Residential developments that have higher densities than the Comprehensive Plan has used to calculate the amount of land needed [Township 7, Valley Farms 1, 2, 3, Redwood Meadows, Country Club Estates Annex No. 4] help to balance out the overall residential density of development.

If the lands that have been subdivided since 1988 were subdivided according to the density that the Comprehensive Plan uses to determine the amount of land needed within the Urban Growth Boundary, the density of the developments would be approximately 6.5 lots per

developable acre. The land that have been subdivided since 1988, have been developed at approximately 5.5 lots per developable acre.

Lands that are zoned R-1, Low Density Residential, that have been subdivided since 1988, have been developed at approximately 5.0 lots per developable acre. The development being proposed in this application will have a higher density than the average for recent developments of the same zone classification.

3. Request for comments have been sent to all public facility and service providers (see discussion under Public Services Element).
4. No natural hazards have been identified on the subject property.
5. The zoning of the property, R-1, Low Density Residential, is not consistent with the Land Use Map designation for the property (Medium Density Residential). However, there is no application for a zone change from the current R-1, Low Density Residential zone. The minimum lot size for parcels in the R-1 zone is 7000 square feet, and all of the proposed lots meet the minimum lot size requirement. In 1988 [ZC 88-03.5] and in 1989 [ZC 89-01.5], the property owner's proposal to rezone this property to R-1.5 was not permitted by the Planning Commission. Only the portion of the tax lots for which a subdivision was then proposed, was rezoned. The remainder of the tax lot was not rezoned as requested.
6. The subject property is identified as one of the "unique" sites or "areas of special concern". The discussion of the Comprehensive Plan's designation for this property includes upzoning of the property. The purpose of the Comprehensive Plan's designation of this property for medium density residential is given in the last sentence of the Comprehensive Plan's discussion of this "unique" area:

"Its proposed density was increased partly to assist the developer in providing affordable housing in spite of the costs of physical improvements which will be needed."

The developer is no longer requesting, or needing, an increase in the density to provide "affordable" housing in spite of the costs of the physical improvements needed.

iv. Environmental Concerns

- **GOALS: TO PROTECT IDENTIFIED NATURAL AND HISTORICAL RESOURCES.**

**TO PREVENT AIR, WATER, LAND, AND NOISE
POLLUTION.**

**TO PROTECT LIVES AND PROPERTY FROM NATURAL
HAZARDS.**

- Policy #1-R-A: Canby shall direct urban growth such that viable agricultural uses within the Urban Growth Boundary can continue as long as it is economically feasible for them to do so.
- Policy #1-R-B: Canby shall encourage the urbanization of the least productive agricultural area within the Urban Growth Boundary as a first priority.
- Policy #2-R: Canby shall maintain and protect surface water and groundwater resources.
- Policy #3-R: Canby shall require that all existing and future development activities meet the prescribed standards for air, water and land pollution.
- Policy #4-R: Canby shall seek to mitigate, wherever possible, noise pollution generated from new proposals or existing activities.
- Policy #5-R: Canby shall support local sand and gravel operations and will cooperate with County and State agencies in the review of aggregate removal applications.
- Policy #6-R: Canby shall preserve and, where possible, encourage restoration of historic sites and buildings.
- Policy #7-R: Canby shall seek to improve the overall scenic and aesthetic qualities of the City.
- Policy #8-R: Canby shall seek to preserve and maintain open space where appropriate, and where compatible with other land uses.
- Policy #9-R: Canby shall attempt to minimize the adverse impacts of new developments on fish and wildlife habitats.
- Policy #1-H: Canby shall restrict urbanization in areas of identified steep slopes.

Policy #2-H: Canby shall continue to participate in and shall actively support the Federal flood insurance program.

Policy #3-H: Canby shall seek to inform property owners and builders of the potential risks associated with construction in areas of expansive soils, high water tables, and shallow topsoil.

ANALYSIS

1-R-A. The property is a part of the Township Village Development master plan. The applicant started the development of their property in 1988 with a master plan for the eventual development of all of their property. The non-developed property is currently used for Christmas tree farming. Upon harvesting of the trees, each portion of land has been planned for development.

1-R-B. With the development of the adjoining land, and the provision of all the necessary public utilities to the subject property, the property is considered to be "urbanized".

2-R. The storm water drainage of the subject property, except for the drainage for the public roads, is handled on-site. Clackamas County reviews storm water management and compliance with the Federal Clean Water Act, for each individual lot upon development of that lot. The street drainage is reviewed by the City.

3-R. The existing use has not created a known pollution problem. Construction activity, the development activity directly related to residential development, is required to comply with prescribed standards for air, water, and land pollution, through the building permit process. Storm water drainage is mentioned in the above 2-R.

4-R. Noise will be expected as a result of residential construction. No residential construction.

5-R. The subject property is not a sand and gravel operation, nor will the proposed partition or future use of the land hinder any sand and gravel operation. There is no sand and gravel operation within the City limits.

6-R. There are no historic buildings on or around the subject property. The subject property and surrounding properties are not historic sites.

7-R. Residential development will affect the scenic and aesthetic quality of the City. Open farm land or, in this case, Christmas tree farm land, is considered to be a positive scenic and aesthetic quality. However, urbanization of land within the Urban Growth Boundary is permitted.

The review of that development takes into consideration the scenic and aesthetic quality of that development. Further, a six (6) acre portion of the overall Township Village development site, which is located to the southeast of the subject property, has been dedicated to the City for park/open space.

8-R. The subject property is considered to be open space at this time. Preservation of the full property in perpetuity is impractical. The dedication of the six (6) acres immediately to the southeast of the subject property has occurred and will preserve a reasonable portion of the overall Township Village development site as open space.

9-R. No wildlife or fish habitats are known on the subject property.

1-H. The subject property has no steep slopes.

2-H. The subject property is not in a flood zone.

3-H. The subject property has Latourell loam soil, which is a deep, well-drained soil. No expansive soils, shallow topsoil, high water table, or other potential risks associated with construction on the subject property have been identified.

v. Transportation

■ **GOAL: TO DEVELOP AND MAINTAIN A TRANSPORTATION SYSTEM WHICH IS SAFE, CONVENIENT AND ECONOMICAL.**

Policy #1: Canby shall provide the necessary improvement to City streets, and will encourage the County to make the same commitment to local County roads, in an effort to keep pace with growth.

Policy #2: Canby shall work cooperatively with developers to assure that new streets are constructed in a timely fashion to meet the City's growth needs.

Policy #3: Canby shall attempt to improve its problem intersections, in keeping with its policies for upgrading or new construction of roads.

Policy #4: Canby shall work to provide an adequate sidewalks and pedestrian pathway system to serve all residents.

- Policy #5: Canby shall actively work toward the construction of a functional overpass or underpass to allow for traffic movement between the north and south side of town.
- Policy #6: Canby shall continue in its efforts to assure that all new developments provide adequate access for emergency response vehicles and for the safety and convenience of the general public.
- Policy #7: Canby shall provide appropriate facilities for bicycles and, if found to be needed, for other slow moving, energy efficient vehicles.
- Policy #8: Canby shall work cooperatively with the State Department of Transportation and the Southern Pacific Railroad Company in order to assure the safe utilization of the rail facilities.
- Policy #9: Canby shall support efforts to improve and expand nearby air transport facilities.
- Policy #10: Canby shall work to expand mass transit opportunities on both a regional and an intra-city basis.
- Policy #11: Canby shall work with private developers and public agencies in the interest of maintaining the transportation significance as well as environmental and recreational significance of the Willamette River.
- Policy #12: Canby shall actively promote improvements to State highways and connecting County roads which affect access to the City.

ANALYSIS

1. The City has adopted a Transportation Master Plan that specifies the City's responsibilities, and acknowledges the County's and State's responsibilities for improving existing roads due to normal travel wear. The Transportation Master Plan also recognizes that as vacant property abutting a road in need of improvement develops, the development is responsible for the necessary improvements. A fence along the boundary between the subdivision and the school property will need to be constructed to control access to the school property, including the boundary along Township Village Phase VI.

2. The extension of S.E. 8th, 9th and 10th Avenues and S. Larch and Lupine Streets is proposed to access the building lots for this subdivision. The construction of these public roads will need to conform to the road construction standards set for local streets.

A connection between S.E. 10th Avenue and S. Ivy Street has been discussed frequently with previous Township Village phases. There have been concerns regarding safety along S. Ivy Street for such an intersection. At the present time, right-of-way is not available across the land between Township Village and S. Ivy Street to build a connection. However, a connection is still, both desired, and shown in the City Council-approved Transportation Systems Plan. Construction of a road connection between S.E. 10th Avenue and S. Ivy Street that terminates short of S. Ivy Street, at this time, is unnecessary. Right-of-way for a future connection is needed. The right-of-way for the S.E. 10th Avenue extension, beyond S. Larch Street, is to be forty (40) feet wide, starting at the intersection of S. Larch Street and S.E. 10th Avenue, and ending at the southwestern corner of the property.

Street trees are required as a part of land division development. The location of the planting of the trees needs to be both compatible with the placement of the utilities for the subdivision and such that the trees have adequate room to grow. With the right-of-way width of forty (40) feet and the paved street width of thirty-six (36) feet, and the provision for sidewalks, there is virtually no extra right-of-way space. In order to accommodate utilities and street trees, with the limited right-of-way space street trees are to be planted eleven (11) feet behind the curb. In order to insure, as best as possible, the proper planting of the trees, the trees will need to be planted by a licensed landscape contractor. The planting of the trees is a subdivision improvement, and improvements are required to be completed or bonded prior to the signing of the subdivision plat. Since the planting of street trees is inadvisable prior to the construction of the homes, the trees will need to be bonded. One other option is for the developer to pay the City to take responsibility for the planting of the trees. This option requires a payment to the City of \$165 a tree. The cost covers both the purchase of the trees and the cost to contract the work of planting the trees. Among the CC&R's that may be filed for the subdivision, the developer will need to include one that specifically permits the street trees to be planted within the utility easement along the street. This is necessary because the trees may not be planted until after the home has been occupied, and the new homeowner should know, at the earliest possible time, that the trees will be planted. One tree per interior lot, and two trees per corner lot is required. The location of the trees will be designated with the construction plans. The type of trees to be planted has been designated by the owner as follows:

S.E. 10th Avenue	Kwanzan Cherry
S.E. 9th Avenue	Krauter's Vesuvius Plum
S.E. 8th Avenue	Yoshino Cherry
S. Lupine Street	Eastern Redbud
S. Larch Street	Summer Snow Japanese Tree Lilac

3. There are two major intersections near the subject property, the intersection of S.E. Township Road (a collector) and S. Ivy Street (an arterial), and the intersection of S.E. 13th Avenue (an arterial) and S. Ivy Street (an arterial). Both intersections are considered to be "problem intersections". The City has adopted a Transportation Systems Development Charge (SDC). This subdivision development will contribute to the improvement of the intersections through the SDC at the time the individual homes are constructed within the subdivision.

4. Sidewalks will be required for both sides of all proposed public roads. The sidewalks are proposed to be five (5) feet wide and placed alongside the curb. The sidewalks will be looped around mailboxes and other obstructions to provide a clear five (5) foot sidewalk. The location of the sidewalks on the existing S. Larch, S. Lupine, S.E. 8th, and S.E. 9th are set in from the curb by approximately two feet. The new sidewalks will need to make the adjustment from the existing sidewalk location to the curb location in a short of distance as can be practically accomplished.

In order to provide unobstructed use of the sidewalk, and to allow for off-street vehicle parking in front of the garage, a minimum distance of nineteen (19) feet will need to be maintained between the back of the sidewalk and the face of the garage, as measured from the outward most facing of the garage. If the newspaper boxes and/or mailboxes are located adjacent to the driveway approach, such that the sidewalk is set back from the curb, the minimum distance is from the back of the sidewalk, as it meets the driveway, that is the closest to the house.

Section 16.64.030(C) of the Land Development and Planning Ordinance states that one walkway is needed for a block length in excess of 800 feet and two walkways are needed for a block length in excess of 1200 feet. The block on the east side of S. Larch Street will be approximately 1080 feet long. One walkway is proposed to be at the southern end of the block. The properties to the west, along S. Ivy Street have been developed, and providing a walkway to the west will be impracticable, except for the proposed location. The block on the south side of S.E. 10th Avenue will be in excess of 1400 feet long. Two walkways are proposed, one approximately 920 feet from S. Pine Street, and the other at the western end of S.E. 10th Avenue (approximately 600 feet west of the first walkway). The first walkway should be moved two lot lines to

the east to center it between the second walkway (740 feet) and S. Pine Street (780 feet). The school property is located to the south along this property boundary. The relocation will not adversely affect the function of the school.

The blocks between S. Lupine Street and S. Pine Street, and between S.E. 7th Avenue and S.E. 8th Avenue, and between S.E. 8th Avenue and S.E. 9th Avenue, will be approximately 850 feet in length. Two walkways will be needed and will need to be offset from each other. The design of offset walkways is to better avoid safety conflicts between vehicles using the street and bicyclist crossing the street between the two walkways. The location of the two walkways will need to be between the lots 132 and 133, and lots 158 and 159; and between lots 163 and 164, and lots 187 and 188.

A ten (10) foot wide walk way is required to provide adequate pedestrian/bike pathways. Fencing of the walkways and concrete or metal centerpost to restrict access onto the walkway is also needed for ensuring that the walkway will be used for the purposes designated, and provide adequate privacy to the adjoining homes.

5. The subject property is not involved in any possible overpass or underpass of Highway 99-E and the railroad.
6. Both the Police Department and the Fire District have responded to the application, and neither emergency service provider indicated any problem with access to the proposed subdivision.
7. The roads proposed in the subdivision are to be local roads. According to the Transportation Master Plan, no special provision for bicycles or slow moving vehicles are considered necessary in the construction of local roads.
8. The existing use and the proposed use of the property have no specific use for the rail facilities that exist in Canby.
9. The proposed subdivision has no bearing on efforts to improve or expand nearby air transport facilities.
10. The mass transit system in operation in Canby has no direct bearing on the proposed partition. No future transit stops have been proposed. The City has undergone a Transportation Master Plan study which includes mass transit considerations.
11. The subject property is not near the Willamette River and will have no effect on the transportation potential or use of the Willamette River.

12. The development does not involve any County or State roadways.

vi. Public Facilities and Services

■ **GOAL:** TO ASSURE THE PROVISION OF A FULL RANGE OF PUBLIC FACILITIES AND SERVICES TO MEET THE NEEDS OF THE RESIDENTS AND PROPERTY OWNERS OF CANBY.

Policy #1: Canby shall work closely and cooperate with all entities and agencies providing public facilities and services.

Policy #2: Canby shall utilize all feasible means of financing needed public improvements and shall do so in an equitable manner.

Policy #3: Canby shall adopt and periodically update a capital improvement program for major City projects.

Policy #4: Canby shall strive to keep the internal organization of City government current with changing circumstances in the community.

Policy #5: Canby shall assure that adequate sites are provided for public schools and recreation facilities.

ANALYSIS

1. All needed public facility and service providers were sent a "Request for Comments" regarding this application. Positive responses have been received from the School District, Northwest Natural Gas, Fire District, Canby Telephone Association, Police Department, and Sewer Department. All have indicated that adequate facilities and/or services are available, or will be made available through the development. The Canby Utility Board and Public Works Department have not responded. There has been no recent indication, unofficial or otherwise, of potential inadequacy of facilities or service from these providers.

Easements for utility construction and maintenance will be required. The Canby Utility Board does not have the information necessary to provide specific comments regarding the water and electric distribution systems within the subdivision. The water and electric utilities, including street lights, need to conform to the Canby Utility Board requirements, and other utility provision and construction will need to conform to the respective utility requirements.

One additional fire hydrant will be required for this subdivision. The location of the hydrant, as determined by the Fire Marshall, will be needed on the common property line of lots 109 and 110.

According to the Canby School District enrollment figures for September of 1994, the school district had capacity for an additional 1213 students. This breaks down to a capacity for 692 students in the K-5 grades, and 521 students in the 6-12 grades.

The figures for the number of children per dwelling unit (broken into the two grade categories) provided by the school district are: K-5 = .36 children per dwelling unit, and 6-12 = .27 children per dwelling unit. There have been a total of 278 units constructed (or under construction) between 3/1/94 and 5/31/95. There are an additional 286 units that have been approved and no building permits have been issued as of 5/31/95. An additional 306 lots are under review (including the 97 lots in this application).

The capacity of the schools, taking into account approved vacant lots and units constructed no more than six months prior to the start of the 1994-5 school year, and then the two additional proposals, is as follows:

REMAINING CAPACITY OF SCHOOLS (June 1995)

	<u>9/94</u>	<u>Approved</u>	<u>Approved + Proposed</u>
K-5	692	489	379
6-12	521	368	286

2. Needed 'public improvements' include street construction with curbs, sidewalks, street trees, street lights, and storm water system. All of these improvements have been discussed under the Transportation Element discussion. Additional improvements include the needed utility infrastructure.

3. The City has adopted a Transportation Systems Development Charge (SDC). The improvements that will be needed for the intersection of S.E. 13th Avenue and S. Ivy Street, and S.E. Township Road and S. Ivy Street, as well as other needed off-site transportation improvements, will be funded by through SDC according to the City's Transportation Systems Plan. This subdivision development will contribute to those improvements through the SDC's at the time that homes are constructed on the subdivision's individual lots.

4. The City's internal organization is not germane to this application.

5. The City has adopted a Parks Master Plan in which appropriate sites or areas for recreation facilities are identified. Park land has been dedicated immediately to the east of the Philander Lee and Ackerman Jr. High campuses. No further dedication is necessary.

vii. Economic

■ **GOAL:** TO DIVERSIFY AND IMPROVE THE ECONOMY OF THE CITY OF CANBY.

Policy #1: Canby shall promote increased industrial development at appropriate locations.

Policy #2: Canby shall encourage further commercial development and redevelopment at appropriate locations.

Policy #3: Canby shall encourage economic programs and projects which will lead to an increase in local employment opportunities.

Policy #4: Canby shall consider agricultural operations which contribute to the local economy as part of the economic base of the community and shall seek to maintain these as viable economic operations.

ANALYSIS

1. The proposed development is not industrial in nature, nor does the current zoning of the subject property allow industrial development.
2. The proposed development is not commercial in nature, nor does the current zoning of the subject property allow commercial development.
3. Development of this site, with homes, will provide residences for Canby business owners and employees, and also will provide a few employment opportunities and expand the market for Canby businesses.
4. The proposed subdivision will have the effect of eliminating, as planned and acknowledged by the City, the Christmas tree farming on the property.

viii. Housing

■ **GOAL:** TO PROVIDE FOR THE HOUSING NEEDS OF THE CITIZENS OF CANBY.

- Policy #1: Canby shall adopt and implement an urban growth boundary which will adequately provide space for new housing starts to support an increase in population to a total of 20,000 persons.
- Policy #2: Canby shall encourage a gradual increase in housing density as a response to the increase in housing costs and the need for more rental housing.
- Policy #3: Canby shall coordinate the location of higher density housing with the ability of the City to provide utilities, public facilities, and a functional transportation network.
- Policy #4: Canby shall encourage the development of housing for low income persons and the integration of that housing into a variety of residential areas within the City.
- Policy #5: Canby shall provide opportunities for mobile home developments in all residential zones, subject to appropriate design standards.

ANALYSIS

1. The location and size of the Urban Growth Boundary is not a part of the proposed application. When the Urban Growth Boundary was designated and calculations to determine the amount of land needed for residential growth, in 1984 as a part of the acknowledged 1984 Comprehensive Plan, the subject property was counted for residential development.
2. The proposed development will increase the overall housing density, as the development's density is at the high end of the potential for the property and higher than the standard that is called for in the Comprehensive Plan.
3. The proposed development does not include higher density housing.
4. The proposed development does not include housing for low income persons.
5. The proposed development is not a mobile home development.

ix. Energy Conservation

■ **GOAL:** TO CONSERVE ENERGY AND ENCOURAGE THE USE OF RENEWABLE RESOURCES IN PLACE OF NON-RENEWABLE RESOURCES.

Policy #1: Canby shall encourage energy conservation and efficiency measures in construction practices.

Policy #2: Canby shall encourage development projects which take advantage of wind and solar orientation and utilization.

Policy #3: Canby shall strive to increase consumer protection in the area of solar design and construction.

Policy #4: Canby shall attempt to reduce wasteful patterns of energy consumption in transportation systems.

Policy #5: Canby shall continue to promote energy efficiency and the use of renewable resources.

ANALYSIS

1. Energy conservation and efficiency as a part of construction practices has been incorporated into the building permit review process and the Uniform Building Code.
2. Of the 97 proposed lots, 89 will meet the basic solar access standards for new residential developments (91.8%). This is well in excess of the minimum 80% required by the solar access ordinance standards for new residential developments.
3. Any building will be required, as a part of the building permit review process, to be reviewed for compliance to the Solar Ordinance.
4. The City's transportation standards, and this development, are in accordance with the City's Transportation Master Plan.
5. Energy conservation and efficiency as a part of construction practices has been incorporated into the building permit review process and the Uniform Building Code.

C. Conclusion Regarding Consistency with the Policies of the Canby Comprehensive Plan and Compliance with Other Applicable City Ordinances:

Review of the above analysis will show that the proposed subdivision, with the recommended conditions of approval, is consistent with the policies of the Comprehensive Plan. Development of each of the lots will need to comply with all applicable provisions of the City of Canby Land Development and Planning Ordinance, Building Codes, and other County and State codes and regulations.

D. Conformance with Applicable Requirements of the Land Development and Planning Ordinance

16.64.010 Streets.

The proposed street names are in accordance with the Street Naming Ordinance. The design of the streets are in compliance with the City's standards. The local roads will be constructed at a paved width of thirty-six (36) feet, with a five (5) foot sidewalk provided against the curb, and utilities and street trees behind the sidewalk.

16.64.030 Easements.

Six foot utility easements will be required to be located along all interior lot lines, with twelve foot utility easements for the rear property and street lot lines.

The sidewalks will be located against the curb. The sidewalks will be "swung" around obstacles (such as mailboxes, newspaper boxes and fire hydrants) that are located against the curb. The width of the sidewalk will be five feet, including the curb when the sidewalk is against the curb. The minimum distance between the garage and the back of the sidewalk is nineteen feet (allowing for a car to be parked in front of the garage without obstructing the sidewalk), regardless of the sidewalk's location. Locating the sidewalk in this manner will allow for the utilities and street trees to be placed with the least amount of conflict and hinderance on the "buildable" area of the lot. In addition, the sidewalk will be kept clear of obstacles to pedestrians.

16.64.040 Lots.

The lots will range in size from approximately 7,000 square feet to approximately 14,500 square feet. All lots will meet the minimum required area of 7,000 square feet, and will be of such dimensions as not to preclude development with single-family homes for reasons of insufficient room for required setbacks. All lots are at least 60 feet in width for interior lots and at least 65 feet for corner lots. All lots are functional for residential uses.

16.64.050 Public Open Spaces.

Six (6) acres of land has been dedicated for park land to the southeast of the development. The dedication was made by the applicant and the land will be used in conjunction with the school properties to the west of the dedicated land.

16.64.070 Improvements.

A bond will be required for any improvements in the subdivision that are not completed prior to the signing of the final plat. Such agreement of assurance shall be in conformance with Paragraph (O) of Section 16.64.070 of the Land Development and Planning Ordinance. A pre-construction conference with the developer, the City, and the utility providers is necessary prior to any construction of the improvements.

- E. Design and Lot Arrangement - The Overall design shall be functional in terms of sites, utility easements and access, without hindering adjacent development.**

The layout and provision of services to the proposed subdivision has been described by staff in detail in the preceding sections (B & D). The design and arrangement of the lots and streets are functional.

IV. CONCLUSION

In general, the proposed subdivision is consistent with the Comprehensive Plan, all applicable requirements of the Land Development and Planning Ordinance, and the overall design and arrangement of lots is functional and will not unduly hinder use or development of adjacent properties. Conditions are proposed herein to provide the necessary changes and details required to meet the City's standards for subdivisions.

V. RECOMMENDATION

Based on the application, site plan, the facts, findings and conclusions presented in this report, and without benefit of public testimony, staff recommends that should the Planning Commission approve, with conditions, SUB 94-05, that the following conditions apply:

For the Final Plat:

1. Twelve (12) foot utility easements shall be provided along all exterior lot lines. The interior lot lines shall have six (6) foot utility easements as proposed.

2. The final plat shall reference this land use application - City of Canby, File No. SUB 94-05, and shall be registered with the Clackamas County Surveyor's Office and recorded with the Clackamas County Clerk's Office. Evidence of this shall be provided to the City of Canby Planning Department prior to the issuance of building permits requested subsequent to the date of this approval.
3. The final plat mylars must contain, in the form specified, all information necessary to satisfy all matters of concern to the County Surveyor, or his authorized Deputy, including, but not necessarily limited to, various matters related to land surveying, land title, plat security, and plat recordation.
4. The walkway that is proposed to be between lots 96 and 97 shall be located between lots 94 and 95. Two additional walkways shall be located as follows:

between lots 132 and 133, and lots 158 and 159; and,
between lot 163 and 164, and lots 187 and 188
5. A forty (40) foot right-of-way shall be platted for the extension of S.E. 10th Avenue to the west. The right-of-way shall start from the intersection of S.E. 10th Avenue and S. Larch Street and shall end at the southwestern corner of the property.

As a part of construction:

6. A pre-construction conference shall be held prior to construction. The pre-construction plans shall be reviewed and approved by the Canby Utility Board, the Canby Telephone Association, and the City prior to the pre-construction conference. The City's review and approval shall be coordinated through the Planning Office. The construction plans shall include the street design, storm water, sewer, water, electric, telephone, gas, street lights, mail boxes and street trees. The street tree planting location shall be determined as a part of the pre-construction review process.
7. Any necessary utilities shall be constructed to the specifications of the utility provider.
8. One additional fire hydrant shall be provided on the common property line of lots 109 and 110.
9. Street name and traffic control signs shall be provided at the developer's expense. This shall include "Stop" street signs where required by the Director of Public Works.
10. Erosion-control during construction shall be provided by following the recommendations of the "Erosion Control Plans Technical Guidance Handbook," as used by Clackamas County, dated August 1991, and as revised.

11. The construction of the sewer system and street storm water system for the subdivision shall be approved by the Public Works Supervisor.
12. The local roads shall be constructed to the City specifications and standards. The improvements shall include the street, curbs, sidewalks, street lights and street trees.
13. The sidewalks shall be located against the curb, and shall be five-feet wide, including the curb. Where mailboxes, newspaper boxes or other obstructions (such as fire hydrants) are located at the curb, the sidewalk shall be set away from the curb such that the sidewalk remains unobstructed for a full five-foot width. The sidewalks that are adjacent to the existing sidewalks shall be adjusted from the existing sidewalk setback of two feet to the new location against the curb. This adjustment shall occur in the shortest distance that is practicable.
14. The walkways shall be paved ten (10) feet wide. A solid or "good-neighbor" wood fence shall be constructed on both sides of each walkway. The fence shall be six (6) feet high, except within twenty (20) feet of the front property lines. Within twenty (20) feet of the front property lines, the fence shall be three and one-half (3-1/2) feet high. A concrete or metal post shall be placed in the center of the entrance to each walkway. The concrete sidewalks shall continue across the front of the entrances, with a handicap accessible ramp to the street. A center post shall be placed at the southwestern end of the walkway that will be constructed in the right-of-way between lots 105 and 106.
15. The type of street trees to be planted are as follows:

S.E. 10th Avenue	Kwanzan Cherry
S.E. 9th Avenue	Krauter's Vesuvius Plum
S.E. 8th Avenue	Yoshino Cherry
S. Lupine Street	Eastern Redbud
S. Larch Street	Summer Snow Japanese Tree Lilac

The trees shall be planted eleven (11) feet from the street curb.

16. A six-foot high, chain-link fence shall be constructed along the subdivision's boundary with the school properties to the south, except for the walkway entries. The fence shall also be constructed along the boundary between Township Village Phase VI and the school properties.

Prior to the signing of the Final Plat:

17. The subdivision development fee, as provided in the Land Development and Planning Ordinance Section 16.68.040(G), shall be paid.

18. The land divider shall follow the provisions of Section 16.64.070 Improvements, in particular, but not limited to, subparagraph (O) Bonds, which requires a surety bond, personal bond, or cash bond for subdivision improvements for any improvement not completed prior to the signing of the final plat. The bond shall provide for the City to complete the required improvements and recover the full cost of the improvements.
19. Included in any Covenants, Conditions, and Restrictions filed with the subdivision, shall be wording that states that street trees are permitted, and will be planted, within the utility easement along the street frontage. If no other CC&R's are filed with the subdivision, then this wording shall be filed individually. A copy of the CC&R's to be filed with the subdivision shall be submitted to the City Planning Department with the final plat prior to the signing of the final plat.
20. One of two options shall be fulfilled for the planting of street trees prior to the signing of the final plat: Option 1; a contract, with a licensed landscape contractor, shall be executed. The contract shall include the City as the contractee: Option 2; the developer shall pay the City \$18,480 for the 112 trees to be planted (\$165 a tree). If option 2 is chosen, the City becomes responsible for the planting of the street trees.

After construction:

21. "As-built" drawings shall be submitted to the City within sixty (60) days of completion. A copy of the "as-built" drawings shall be submitted on a computer disk in an autocad format.
22. Garages shall be set back a minimum of nineteen (19) feet from the back of the sidewalk. The distance shall be measured from the closest edge of the sidewalk at the driveway.
23. The final plat must be submitted to the City within one (1) year of the approval of the preliminary plat approval according to Section 16.68.020.
24. The approval will be null and void if the final plat is not submitted to the County within six (6) months after signing of the plat by the chairman of the Planning Commission (Section 16.68.070).

Exhibits:

1. Application
2. Vicinity Map
3. Tentative Plat (*too large to reproduce*)
4. Responses to Request for Comments

SUBDIVISION APPLICATION

Fee: \$900 + \$50/lot

OWNER

APPLICANT

Name Regan Enterprises Address 31233 French Prairie Rd. City Wilsonville State OR Zip 97070 SIGNATURE Joseph E. Regan Phone: 694-5454

DESCRIPTION OF PROPERTY: TL700 R41E04A & TL4800 R41E04AA

Tax Map 4-1E-4AA Tax Lot(s) 4800 Lot Size 17.61 (Acres/Sq. Ft)

or

Legal Description, Metes and Bounds (Attach Copy)

Plat Name Lot Block

PROPERTY OWNERSHIP LIST

Attach a list of the names and addresses of the owners of properties located within 200 feet of the subject property...

USE

Existing Vacant-Tree Farm Proposed Single Family Residential

Existing Structures

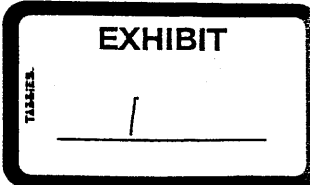
PROJECT DESCRIPTION

Owner proposes 97 single family residential lots. See narrative on tentative plat.

ZONING R-1 COMPREHENSIVE PLAN DESIGNATION R-1

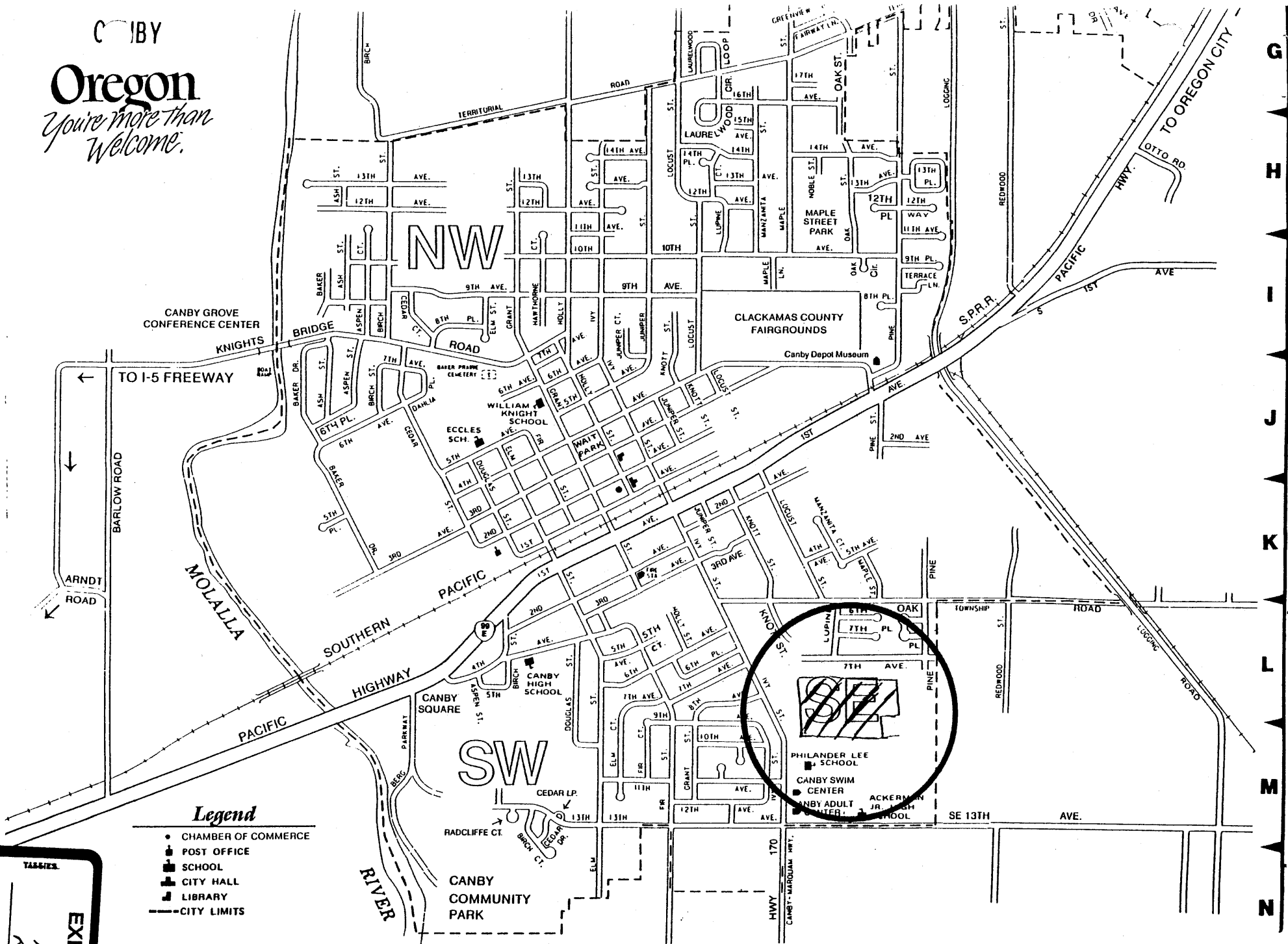
PREVIOUS ACTION (if any) Annexation 90-02

File No. SUB 95-04 Receipt No. 9464 Received by JSW Date Received 5/29/95 Completeness Date Pre-App Meeting Hearing Date 6/26/95



If the applicant is not the property owner, he must attach documentary evidence of his act as agent in making application.

CANBY
Oregon
*You're more than
 Welcome.*



Legend

- CHAMBER OF COMMERCE
- POST OFFICE
- SCHOOL
- CITY HALL
- LIBRARY
- CITY LIMITS

TABULET
 2
EXHIBIT

PLEASE RETURN ATTACHMENTS!!!

CANBY PLANNING DEPARTMENT REQUEST FOR COMMENTS

P.O. Box 930, Canby, OR 97013

[503] 266-4021

DATE: June 1, 1995

TO: FIRE, POLICE, CUB, SCHOOLS, TOM PIERSON/ TODD SCHMIDT, NW NATURAL GAS,
CANBY POST OFFICE, MIKE JORDAN, JOHN KELLEY, ROY, STEVE

The City has received SUB 95-04, an application by Regan Enterprises for approval to develop Phase 8 of Township Village, a 97-lot subdivision. The site is located south of S.E. 7th Avenue, east of S. Pine Street [Tax Lot 4800 of Tax Map 4-1E-4AA].

We would appreciate your reviewing the enclosed application and returning your comments by **June 12, 1995 PLEASE**. The Planning Commission plans to consider this application on June 26, 1995. Please indicate any conditions of approval you may wish the Commission to consider if they approve the application. Thank you.

Comments or Proposed Conditions:

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: Don Peterson

Date: June 9 1995

TABLET	EXHIBIT
	<u>4</u> 8 pages

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Comments or Proposed Conditions:

Canby FD #62 request that an additional fire hydrant be placed
on lot 110 and to the SE Corner of the lot.

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will **become available** through the development
- Conditions are needed, as indicated
- Adequate public services are **not available** and will not become available

Signature:  Date: 6-12-95

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Comments or Proposed Conditions:

None

Please check one box:

- Adequate Public Services (of your agency) are available
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Signature: S. Jordan

Date: 6-2-95

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[503] 266-4021

DATE: June 1, 1995

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Conditions are needed, as indicated

Adequate public services are not available and will not become available

Signature: Tom Pierson

Date: 6/5/95

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DATE: June 1, 1995

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Comments or Proposed Conditions:

Park sits out by itself - should be part of development so residences can help keep it's use monitored. This would help us tremendously.

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: *J. Viper*

Date: 6/2/95

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Signature: Stephen Miller, Sup Date: 6-2-95

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Signature: *Gary F. Hyatt* Date: 6/8/95

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Comments or Proposed Conditions:

*Please examine 10th St. access onto Ivy question
and since this appears to be the last phase, please make
sure all requirements for subdivision as a whole are met.*

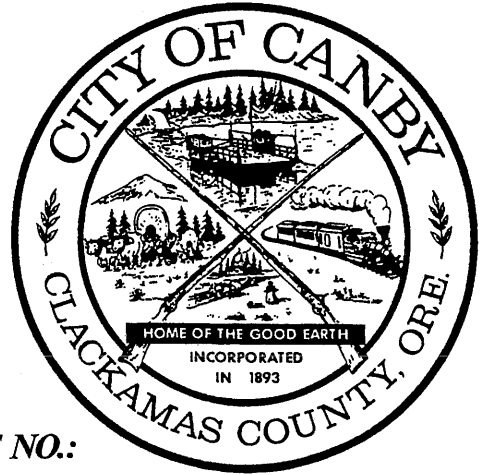
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Signature: Michael Jordan

Date: 6-2-95

- STAFF REPORT -



APPLICANT:

Peck, Smiley, Ettlín Architects
1220 SW Morrison St., Suite 600
Portland, OR 97205

FILE NO.:

DR 95-11
(Canby Fire District)

OWNER:

Canby Fire District No. 62
P.O. Box 909
Canby, OR 97013

STAFF:

James S. Wheeler
Assistant Planner

LEGAL DESCRIPTION:

Tax Lot 804 of Tax Map 3-1E-33DA

DATE OF REPORT:

June 16, 1995

LOCATION:

West side of S. Pine Street, between
S.E. 2nd Avenue and S.E. 3rd Avenue

DATE OF HEARING:

June 26, 1995

COMP. PLAN DESIGNATION:

Highway Commercial

ZONING DESIGNATION:

R-1 (Low Density Residential)

I. APPLICANT'S REQUEST:

The applicant is requesting site and design approval to construct a 14,215 square foot one-story masonry fire station with double loaded 5-bay apparatus room, administrative offices, living quarters, and meeting space. The development is proposed to occur on an 88,336 square foot parcel.

II. APPLICABLE REGULATIONS

- **City of Canby General Ordinances:**
 - 16.10 Off-Street Parking and Loading
 - 16.16 R-1 - Low Density Residential Zone
 - 16.49 Site and Design Review
 - 16.88 General Standards

III. MAJOR APPROVAL CRITERIA

Site and Design Review

The Planning Commission, sitting as the Design Review Board, shall, in exercising or performing its powers, duties or functions, determine whether there is compliance with the following:

- A. The proposed site development, including the site plan, architecture, landscaping and graphic design, is in conformance with the standards of this and other applicable City ordinances insofar as the location, height and appearance of the proposed development are involved; and
- B. The proposed design of the development is compatible with the design of other developments in the same general vicinity; and
- C. The location, design, size, color and materials of the exterior of all structures and signs are compatible with the proposed development and appropriate to the design character of other structures in the same vicinity.

The Design Review Board shall, in making its determination of compliance with the requirements set forth, consider the effect of its action on the availability and cost of needed housing.

IV. FINDINGS:

A. Background and Relationships:

The applicant is requesting approval to construct a 14,215 square foot fire station, with offices, living quarters, and a meeting room. The use is permitted outright in the R-1 zone as a public building. Approximately 22,000 square feet of the parcel (the southern portion of the parcel) will remain undeveloped.

The property is located on the west side of S. Pine Street, between S.E. 2nd Avenue and S.E. 3rd Avenue. Both 2nd and 3rd are streets that "run" east of S. Pine Street. The property is a part of the benefitted area of the Logging Road Industrial Park Road Project. The reimbursement charge for the advanced financed public improvement (sewer, water, and S. Pine Street improvement) will need to be paid.

The property was partitioned (MLP 93-08, approved 3/29/94).

B. Evaluation Regarding Site and Design Review Approval Criteria

1. Part IV - Section 2, No. 2

"Minimum area for landscaping is 15% of the total area to be developed."

The minimum amount of landscaping required for the 66,405 square foot developed portion of the parcel is 19,922 square feet (30%). The total amount of landscaping proposed in phase one is approximately 23,027 square feet (34.7%, including the street side landscaping along S. Redwood Street). The remaining 21,931 square feet of undeveloped area will be left in its current condition.

2. Parking.

There is no specific number of parking spaces given as a requirement for a fire station in the City's Codes. There are nine beds proposed in the bunk rooms, which would, conceivably require nine parking spaces. Another 3,560 square feet (approximately) of office space is proposed, requiring 12.5 parking spaces. The remaining building area is proposed for vehicle/equipment storage, building mechanical rooms, and "living area" for those firemen bunking at the station. Therefore, the minimum parking spaces required would be 22. There are 34 parking spaces proposed. The excess of parking spaces is being proposed for the volunteer fire fighters that come to the station during a "call". While it is generally desirous to minimize parking areas and therefore spaces, staff believes that the number of parking spaces proposed in this application is justified.

The northern parking spaces will be used primarily by volunteers during a fire "call". The southern parking spaces will be used primarily by visitors and staff.

Two handicap parking spaces are required, of which, one is required to be "van accessible". There are two proposed, and both will be "van accessible", as they will share the same access aisle.

No loading berths are required. There are five (5) bay doors which will be used by the emergency vehicles.

3. Access

Access to the proposed development will be from S. Pine Street. No additional improvements are needed along S. Pine Street, with the exception of street trees.

There are three driveways proposed. Two driveways will be for regular passenger vehicles, while the third will only be for the emergency vehicles. The two passenger vehicle driveways will also be used for returning emergency vehicles.

The southern passenger vehicle driveway is located at the beginning of the curve from S. Pine Street to S.E. 3rd Avenue. The northern passenger vehicle driveway is located across from the northeastern corner of the intersection of S.E. 2nd Avenue and S. Pine Street. The northern edge of the emergency vehicle drive is located approximately forty (40) feet south of the centerline of the intersection of S.E. 2nd Avenue and S. Pine Street. All of the driveways are at least forty (40) feet apart from each other. The emergency vehicle driveway width is eighty (80) feet, well in excess of the standard maximum width of forty (40) feet.

The traffic engineering firm of Kittleson & Associates has addressed these driveway factors. In the report (exhibit 3), the report, commenting on the location of the northern driveway in relation to the intersection of S.E. 2nd Avenue and S. Pine Street, states that "no operational or safety problems are expected between this street and the fire station access." Additionally, the report, in commenting on the emergency vehicle driveway width, states that "a curb opening of less than the 80 feet will result in problems and delay for emergency vehicles leaving the station for any emergency response."

The report made the following recommendations:

1. Approve a variance to allow an 80 foot driveway on Pine Street to serve only emergency vehicle access from the station. This is nearly identical to a fire station access onto Highway 214 in Woodburn which carries over 15,000 vehicles per day. Pine Street carries less than 1,000 vehicles per day and is forecast to carry less than 4,000 vehicles per day in the future.
2. Approve the construction of the two driveways on either side of the station which allows internal circulation for fire personnel as well as emergency vehicles returning to the station.

3. Provide the fire station sign on both approaches to the fire station of Pine Street. These signs will be placed in accordance with the Manual on Uniform Traffic Control Devices (MUTCD Sign number W11-8).
4. Provide emergency vehicle pre-emption capability in the traffic signal to be installed on Highway 99-E at Pine Street.

A variance, as a land use action, has not been requested. According to the City Attorney, Section 16.10.070.B.7 ("The standards set forth in this ordinance are minimum standards for access and egress, and may be increased through the site and design review process in any particular instance where the standards provided herein are deemed insufficient to protect the public health, safety and general welfare.") allows the Planning Commission to increase the permitted driveway width, if adequate findings are made to justify the increase. Staff believes that adequate findings have been made to justify the eighty (80) foot driveway width for emergency vehicle egress only.

The traffic light for the intersection of S. Pine Street and Highway 99-E is scheduled to be completed by the end of 1995. The City does not have full control over the light project for the intersection of Highway 99-E and S. Pine Street. Staff is in full agreement with the recommendation of the traffic engineer that an emergency vehicle preemption should be installed for this signal, when it is constructed. A note in the approval of the application (should the Planning Commission approve the application) recommending installation of the emergency vehicle preemption signal is staff's recommendation.

There is concern regarding the safety of emergency vehicles entering the Highway at a non-signalized intersection. The traffic engineer states that a preemption signal would make the intersection safe for emergency vehicle access to the highway. While this does not directly address the concern of operation the fire station at this location prior to the signal light being installed at the intersection of Highway 99-E and S. Pine Street, it appears that emergency vehicle access to the highway at a non-signalized light would be unsafe. Staff finds that the fire station should not be occupied prior to the construction and operation of the traffic signal at Highway 99-E and S. Pine Street.

4. **Architecture**

Painted, split-face concrete block will be the primary material for the building. The roof will be flat and enclosed by a parapet wall. The color will be primarily tan, with the curved sign wall being a reddish brick color. The applicant's narrative gives further details regarding the architectural features of the proposed building.

The applicant has proposed a large sign on a curved wall located on the buildings southeast corner. As proposed, the sign would cover approximately 270 square feet, well in excess of the maximum sixty (60) square feet permitted in a residential zone. At staff's suggestion, an alternative signage pattern has been proposed as well. In the alternative design, the actual signage area on the curved wall will total approximately sixty (60) square feet in size, covering the two ends of the curved wall. While the large curved sign has aesthetic appeal, it is not a necessity for the function of the station, and it is not permitted according to the Sign Ordinance.

5. **Other Aspects**

a. **Utilities**

Service providers have not indicated that there would be any problem in servicing this proposal. To coordinate efforts for construction activity with the utility providers, developers, and City departments, a pre-construction conference will be necessary.

The storm water design and construction for the parking lot and vehicular maneuvering areas may need some alteration. The Public Works Department believes that, as designed, the drywells will be quite a bit deeper than what is considered to be standard. The sewer lateral to the building will need to be six (6) inches in diameter to a point ten (10) feet inside the property line, and a second cleanout will be required within five (5) feet of the foundation of the building. These lateral and cleanout requirements are according to the City's standards.

There will be four lights on both sides of the building along the bay doors. The lights will be 150 watts, mounted at a twelve (12) foot height. The design and mounting of the lights is such that it will illuminate only the driveway approaches to the building. The specific lighting information is in the application file. There will be one, fifteen (15) foot high light for each parking lot. The

parking lot lights will be facing the building, so as to minimize any "spill-over" lighting of residential property to the west.

All infrastructure is available in S. Pine Street.

b. Landscaping

The landscaping is primarily surrounding the building and the parking lot. The landscaping design is quite extensive. Lawn will be used in front of the building, with some building accent shrubs. The parking lots will have entrance accent trees, Chanticleer Pears, and thirteen Flame Ash trees throughout the parking/vehicle maneuvering areas. The shrubs bordering the parking lots will be 'Otto Luyken' laurels, Winged Euonymus, and Barberry. The groundcover will vary between grass, nandina, kinnikinnick, and juniper. The drives to the back of the building will be bordered by mugo pines, barberry, and 'Otto Luyken' laurel, with kinnikinnick, cotoneaster, grass, and juniper groundcovers. The western boundary, between the drive and the manufactured home park to the west, will have a 6-foot chain link fence with slats, and primarily mugo pines and barberry shrubs.

The landscaping around the perimeter of the building will include: two specimen trees (special trees for accent), a magnolia and a Japanese Maple; azaleas, rhododendrons, vine maples (small, multi-stemmed trees), euonymus, viburnum, cypress, and holly; and kinnikinnick, grass and cotoneaster groundcover.

The existing utility transformers in front of the building will be landscaped with Oregon Grape and euonymus.

The northwestern corner of the lot will have a larger landscaped area that will include Shore Pines, California Wax Myrtles (also used along the northern property boundary toward S. Pine Street), cotoneaster (non-groundcover variety), with juniper and cotoneaster groundcovers.

The courtyard will be hardsurfaced, with six flowering cherry trees. The planters for the cherry trees should be at least four feet by four feet in size.

The listing of the plants and the plant spacing proposed generally meets the ODOT minimum plant spacing requirements that the

City uses. Whether or not the sizes of the plants to be planted meets the ODOT minimum standards needs to be verified. The Kinnikinnick and Dammeri Cotoneaster groundcover may need to be planted more closely together than proposed. The proposed plant sizes may be larger than the minimum required, thereby allowing wider plant spacing. There is one Japanese Maple that is proposed. The size of the Maple is 1-1/2" caliper. There are no specific standards for tree sizes outside of the street trees. The unofficial standard has been 2" caliper trees. Japanese Maples are smaller trees and a 2" caliper tree will be fairly expensive in relation to other 2" caliper trees. Staff finds that the one 1-1/2" caliper Japanese Maple tree will not compromise the standards for landscaping that the Planning Commission has been looking for.

Street trees are required. There is sufficient amount of road frontage to provide street trees without compromising emergency vehicle access safety. The standard plant spacing for the Cleveland Select Norway Maples is fifty (50) feet. However, due to the special nature of this development, only four (4) street trees should be planted, instead of eight (8) trees that would be required for the nearly 400 feet of road frontage. The location of the street trees should be as follows: 1) within twenty (20) feet of the southeastern property line's intersection with S. Pine Street; 2) approximately forty (40) feet north of the southern passenger vehicle driveway approach; 3) approximately thirty-five (35) feet north of the northern edge of the emergency vehicle driveway approach; and 4) within thirty (30) feet of the northern property line's intersection with S. Pine Street. These location will provide sufficient clearance from driveway approaches and sufficient vision for the proposed signage (the alternative version of the signage).

The Chanticleer Pear proposed along the southern edge of the northern passenger vehicle entrance will be too close to the street tree and should be moved to the southwest, closer to the building. The mature crown widths of the Norway Maple and the Pear necessitate a planting distance of thirty (30) feet between the trees. The Chanticleer Pears have been proposed to be "entrance pieces" for the parking lots. Relocation of the one pear will create an imbalance. The northern pear should also be relocated to the northwest to keep the balance, although doing this will remove the pears as "entrance pieces" for the northern parking lot.

c. **Parking Lot Landscaping**

The amount of paved area for parking and vehicle maneuvering area is 29,163 square feet. The amount of landscaping required for that amount of area is 4,375 square feet (15%), and is to be within ten feet of the parking/maneuvering area. A rough calculation of the amount of landscaping provided within ten feet of the parking/maneuvering area is approximately 9,090 square feet (31.2%). At the formula of one tree per 2800 square feet of paved vehicular maneuvering and parking area, a total of 11 trees are needed. There are 19 trees within or adjacent to the paved vehicle parking/maneuvering area, not including conifers, vine maples, specimen trees, or street trees.

d. **Density and yards and height**

The setbacks and the height requirements for the R-1 zone have been met by this development proposal.

V. CONCLUSION

The staff hereby concludes that, with appropriate conditions, the proposed development as described in the application, site plan, and this report, is in conformance with the standards of this and other applicable ordinances; the design is compatible with the design of other developments in the vicinity; and, the location, design, size, and materials of the exterior of the structure will be compatible with the proposed development and appropriate to the design character of other structures in the same vicinity.

Further, staff concludes that, with approval conditions:

1. the proposed development of the site is consistent with the applicable standards and requirements of the Canby Municipal Code and other applicable City ordinances insofar as the location, height and appearance of the proposed development are involved; and
2. the proposed design for the development is compatible with the design of other developments in the same general vicinity; and
3. the location, design, size, color and materials of the exterior of all structures and signs are compatible with the proposed development and appropriate to the design character of other structures in the same vicinity; and

4. the conditions listed are the minimum necessary to achieve the purposes of the Site and Design Review Ordinance, and do not unduly increase the cost of housing.

VI. RECOMMENDATION:

Based upon the application, elevations, the site plan received by the City, the facts, findings and conclusions of this report, and without the benefit of a public hearing, staff recommends that should the Planning Commission approve DR 95-11, the following conditions apply:

Prior to the Building Permit Application:

1. A preconstruction conference shall be held prior to the issuance of the building permit. The conference shall be coordinated through the Planning Office.

For the Building Permit Application:

2. A detailed landscape construction plan shall be submitted with the building permit. The detailed landscape plan shall show: the number of plants, plant spacing/location of planting, the type of plants, the size of plants, the schedule of planting, and irrigation plans.
3. The landscaping shall be planted at such a density so as to provide a minimum of 95% coverage of the landscape areas with vegetation, within a 3-year time period. Bark mulch and similar material shall consist of not more than 5% of the total landscape area after the 3-year period. The plant spacing and starting plant sizes shall meet the ODOT plant spacing/starting size standards. The Kinnikinnick and cotoneaster groundcovers shall be planted 2' o.c. and 4' o.c., respectively, unless the size of plants to be planted warrants a wider planting spacing.
4. Four (4) 'Cleveland Select' Norway Maples shall be planted along S. Pine Street. The Maples shall be planted as follows: 1) within twenty (20) feet of the southeastern property line's intersection with S. Pine Street; 2) approximately forty (40) feet north of the southern passenger vehicle driveway approach; 3) approximately thirty-five (35) feet north of the northern edge of the emergency vehicle driveway approach; and 4) within thirty (30) feet of the northern property line's intersection with S. Pine Street. All street trees shall be planted six feet behind the sidewalk, and shall be at least 2" caliper trees when planted.

5. The Chanticleer Pear trees for the northern parking lot entrance shall be moved to the southwest and northwest, respectively. The new location shall be at the end of the first parking spaces on either side of the entrance.
6. The planter sizes for the courtyard cherry trees shall be a minimum of four (4) feet by four (4) feet.
7. The emergency vehicle driveway approach shall not be larger than eighty (80) feet.
8. The smaller, alternative signage design is approved for the fire station. The total signage area is not to exceed sixty (60) square feet.
9. The sanitary sewer lateral shall be six (6) inches in diameter up to ten (10) feet inside the property line. A second sanitary sewer cleanout shall be placed five (5) feet from the foundation of the building.

Prior to Occupancy:

10. The traffic signal at the intersection of Highway 99-E and S. Pine Street shall be constructed and operational prior to occupancy of the fire station.
11. Fire station signs shall be provided on both approaches to the fire station of Pine Street. These signs shall be placed in accordance with the Manual on Uniform Traffic Control Devices (MUTCD Sign number W11-8).

Other Notes:

12. The Planning Commission strongly recommends that the construction of the traffic signal at the intersection of Highway 99-E and S. Pine Street include the capability for emergency vehicle preemption.
13. The advanced financing reimbursement charge will be due upon issuance of the building permit. The principle amount owed is \$5,723.79. Interest accrues at \$0.954 per day. The amount owed as of June 15, 1995, is \$5,998.53. The principle plus interest can be paid at any time up to the issuance of the building permit.
14. The storm water drainage system may need to be redesigned to avoid future drainage problems. The Public Works Department has stated that they will not be willing to help correct any future problems with the storm water drainage system if it is constructed as proposed.

Exhibits:

1. Application for Design Review
2. Vicinity Map
3. Traffic Engineering Report
4. Site Plan/Elevations/Landscape Plan *(too large to reproduce)*
5. Department Responses to "Request for Comments"

SITE AND DESIGN REVIEW APPLICATION

Fee: \$750

OWNER

APPLICANT

Name Canby Fire District No. 62

Name Peck Smiley Ettlin Architects

Address P.O. Box 909

Address 1220 SW Morrison St., Ste. 600

City Canby State OR Zip 97013

City Portland State OR Zip 97205

SIGNATURE [Signature]

Phone: (503) 248-9170

for Canby Fire District.

DESCRIPTION OF PROPERTY:

Jack Stark CFD

Tax Map 3 1E 33DA Tax Lot(s) 804 Lot Size 2.00 Acres
(Acres/Sq. Ft.)

or

Legal Description, Metes and Bounds (Attach Copy)

Plat Name _____ Lot _____ Block _____

PROPERTY OWNERSHIP LIST

Attach a list of the names and addresses of the owners of properties located within 200 feet of the subject property (if the address of the property owner is different from the situs, a label for the situs must also be prepared and addressed to "Occupant"). Lists of property owners may be obtained from any title insurance company or from the County Assessor. If the property ownership list is incomplete, this may be cause for postponing the hearing. The names and addresses are to be *typed onto an 8-1/2 x 11 sheet of labels*, just as you would address an envelope.

USE

Existing Vacant
Proposed Fire Station

Existing Structures None

Surrounding Uses Residential, Light Industrial

PROJECT DESCRIPTION One story masonry fire station with double loaded five bay apparatus bay, administrative offices, living quarters, and meeting space. Area of building is 14,215 Sq. Ft.

ZONING _____ COMPREHENSIVE PLAN DESIGNATION _____

PREVIOUS ACTION (if any) _____

File No. DR 95-11
Receipt No. 9468
Received by [Signature]
Date Received 5-31-95
Completeness Date _____
Pre-App Meeting _____
Hearing Date 6-26-95

* If the applicant is not the property owner, he must attach documentary evidence of his act as agent in making application.

EXHIBIT
1
3 pages



**CANBY FIRE DISTRICT NO. 62
MAIN FIRE STATION
DESIGN REVIEW NARRATIVE**

31 May 1995

PROJECT SUMMARY

This submittal is for the construction of a new main fire station for Canby Fire District No.62. The new fire station will be a replacement facility for the relocation of the existing District No. 62 station. Public meeting space is proposed within the new facility for the use of the residents of the City of Canby. The site was selected to provide the necessary fire protection and emergency response capability for the entire fire district.

The 88,336 sq.ft. site is zoned R-1 and does not require a conditional use approval for the new main fire station. The contract area of the site is 66,405 sq.ft. The remainder of the site will remain unimproved. The site is presently devoid of any structures or improvements.

The new main fire station is 14,215 sq.ft. and will provide a double-loaded five-bay apparatus room and living quarters for up to three shifts of three fire fighters. The station will provide offices for the administrative function of the Canby Fire District No. 62, allowing convenient public access for a variety of services offered by the fire department.

BUILDING DESIGN

The site for the new fire station is at the edge of a residential area that is adjacent to an industrial zone in Canby. Located on Pine street across from 2nd Ave., the site slopes gradually 2% south toward the bottom of the site. There are currently no structures on the site. A mobile home park occupies the lots to the west of the site. These lots are shielded from the fire station site by a large row of trees between the two lots. Across Pine Street to the east, a light industrial facility and a residence exist. There is a residence to the north and south of the site. All of these adjacent structures are single-story.

The proposed new main fire station is designed to provide building massing, shape, proportions, and materials appropriate to the location of acting as a transition from a residential zone to an industrial zone. The design will help the station be a good neighbor both figuratively and literally. Painted split-face concrete block is the primary material of the new facility with painted steel lintels and canopies providing an accent. The top coursing of the block for the apparatus bays have turned vertically to form a soldier course providing a cap. to the bays. The roofs are all flat and hidden behind parapet wall. The

ARCHITECTS

1220 sw Morrison
Suite 600

Portland OR 97205

503 . 248 . 9170

fax 503 . 248 . 0223

Page 2

overhead doors at the apparatus bays will be 12' x 12' with two rows of windows, to allow the practical element of natural lighting, but also to provide a view from Pine Street to the passing pedestrian and motorist. Other exterior doors will be painted hollow metal.

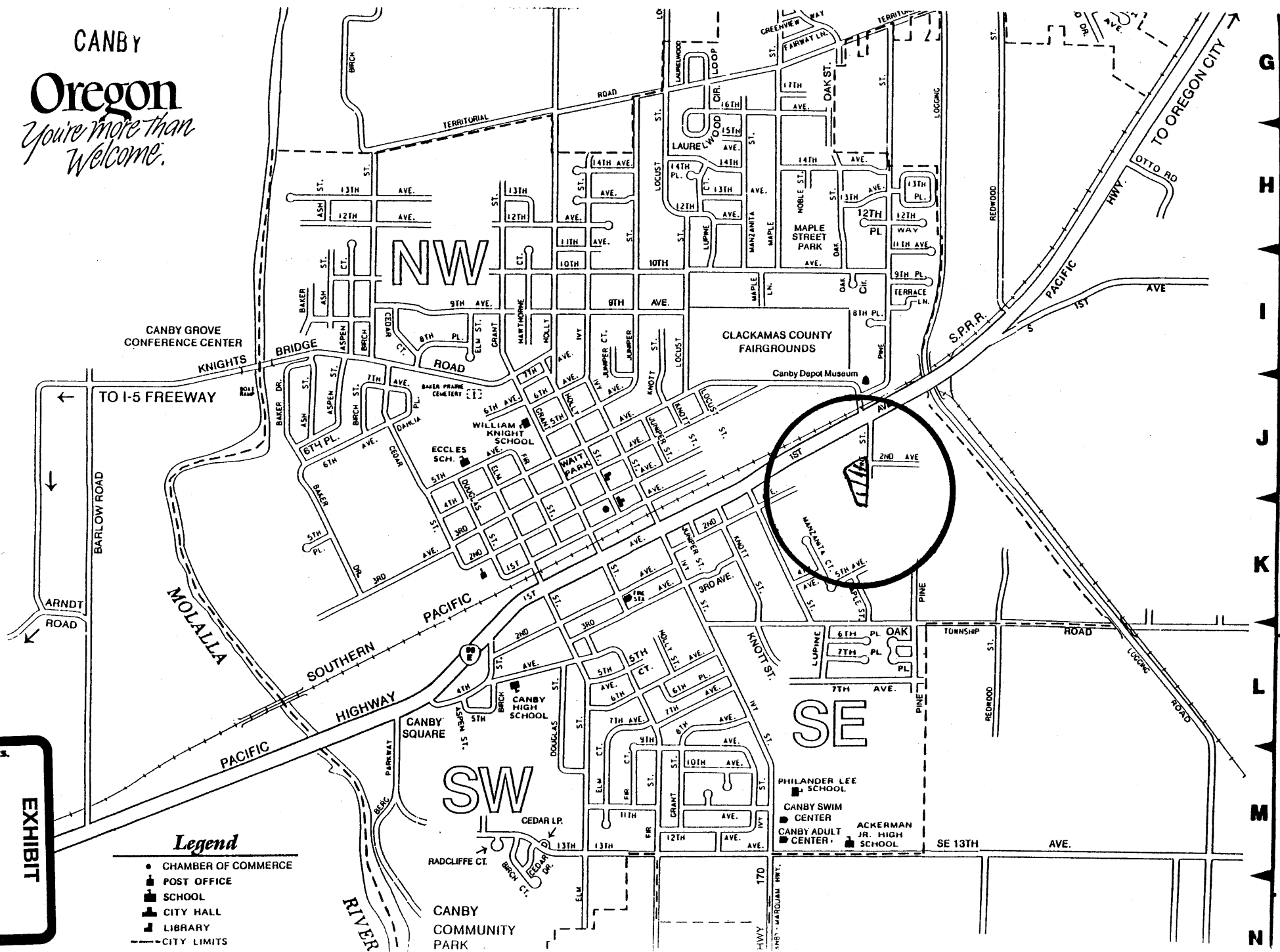
The western corners of the living quarters, the Day Room and the Exercise Room are lower forms designed to break up the mass of the masonry building and produce a lighter scale. An aluminum curtain-wall system and metal panels provide a transparent and open feeling in these active spaces.

A curved stucco wall acts as the "front" facade for those driving north on Pine. The wall is designed to frame the entry canopy, providing a strong sense of arrival, and provide spatial closure for the outdoor plaza as well as being the background for the signage. The signage is designed integrally with the curved wall so that the letters are more important to the building as ornament and as rhythm along the wall than as the actual verbiage of the sign. The wall will be washed with lights mounted in the ground at the base of each of the columns. A steel entry canopy extends through the curved wall to the parking area.

An outdoor plaza will be provided adjacent to the meeting room to allow public events to spill outside in times of good weather and as a location for ceremonies held by the fire department. The plaza is also adjacent to the apparatus bays, which will allow the firemen to pull out a truck for grade school class trips and other demonstrations. The flag pole will be located at the end of the curved wall and the historic bell located at the existing fire station will be placed in the plaza. Five small deciduous trees will provide shade for the space.

The building will be 21'-9" high at its highest point. The building mass of the fire station will provide a comfortable addition to the neighborhood fabric without overwhelming the nearby structures. The new main fire station is a very simple and efficient design that will contribute positively to the civic identity of the City of Canby.

CANBY
 Oregon
*You're more than
 Welcome.*



CANBY GROVE
 CONFERENCE CENTER

CLACKAMAS COUNTY
 FAIRGROUNDS

Canby Depot Museum

WILLIAM KNIGHT
 SCHOOL

ECCLES SCH.

CANBY HIGH SCHOOL

PHILANDER LEE
 SCHOOL

CANBY SWIM
 CENTER

CANBY ADULT
 CENTER

ACKERMAN
 JR. HIGH SCHOOL

SW

SE

NW

MOLALLA

RIVER

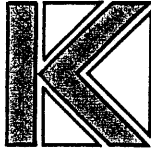
Legend

- CHAMBER OF COMMERCE
- POST OFFICE
- SCHOOL
- CITY HALL
- LIBRARY
- CITY LIMITS

CANBY
 COMMUNITY
 PARK



TARMER.
 EXHIBIT



KITTELSON & ASSOCIATES, INC.
TRANSPORTATION PLANNING/TRAFFIC ENGINEERING

610 S.W. ALDER, SUITE 700 • PORTLAND, OR 97205 • (503) 228-5230 • FAX (503) 273-8169

June 2, 1995

Robert Hoffman, AICP
Planning Director
City of Canby
P.O. Box 930
Canby, OR 97013

Project No.: 1693.00

RE: Analysis of Proposed Fire Station Site/Access on Pine Street - Canby, Oregon

Dear Bob:

An analysis of the proposed siting of a fire station in Canby has been completed as requested. The proposed fire station is to be located on the south side of Pine Street and approximately 300 feet east of Pacific Highway East (99E). Currently, this site is vacant and is located on the recently reconstructed section of Pine Street.

The purpose of this analysis was to review the proposed access in terms of safety and operation on Pine Street and at the intersection of Highway 99E.

The proposed fire station site is planned with three driveways accessing Pine Street. The first driveway would be located 370 feet east of Highway 99E and will provide a 25 foot curb cut to a parking area for the fire personnel. A second driveway would be located approximately 55 feet east of the first driveway and is planned to be an 80 foot curb opening to provide access from the four-bays of the fire station. This driveway will be an entrance-only onto Pine Street from the fire station and only used by emergency vehicles leaving the fire station. The third driveway is proposed to be located 80 feet east of the emergency vehicle access and is planned to be 25 feet in width. This third driveway would provide access to the parking area long the east side of the building as well as access around the building allowing full circulation behind the building and to the first driveway location.

The City of Canby access ordinance allows a driveway width of up to 40 feet. The 80 foot emergency access is significantly larger than currently allowed. The Canby Fire Station proposal is very similar to a fire station in Woodburn located on Highway 214 east of I-5 which has been in place for nearly 20 years. In conversation with the City of Woodburn officials, it was found that the City of Woodburn has an ordinance which allows a curb cut up to 36 feet. The fire station in Woodburn was sited with a variance to the ordinance for an 84 foot curb opening for the purpose of the specific emergency use. No safety or operational problems have been encountered at this site.

An 80 foot curb opening is required at the proposed Canby Fire Station to provide access to Pine Street from all emergency vehicle bays. A curb opening of less than the 80 feet will result in problems and delay for emergency vehicles leaving the station for any emergency response. Problems, such as emergency vehicles not accessing Pine Street at a 90 degree angle will result in increased delay and decrease safety not only for the emergency vehicles but to pedestrian and motorists in the area.

A public right-of-way exists on the opposite side of Pine Street from the first (westerly) fire station driveway. This street will serve the land which is currently zoned industrial on the north side of Pine Street. No operational or safety problems are expected between this street and the fire station access.

The Oregon Department of Transportation (ODOT) has programmed funds for the installation of a traffic signal on Highway 99E at Pine street. This traffic signal should be constructed with the capability of providing fire engine pre-emption to allow the emergency vehicles safe access onto Highway 99E.

Advanced signing on the Pine Street approach to the fire station must be provided in both directions of travel. The sign for westbound travel on Pine Street should be located northeast of the 90 degree curb on Pine Street.

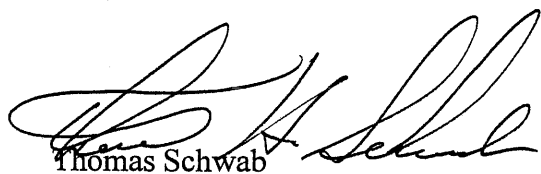
Conclusion/Recommendation

The following recommendations are offered as a result of an analysis of the proposed fire station on the south side of Pine Street in the City of Canby.

1. Approve a variance to allow an 80 foot driveway on Pine Street to serve only emergency vehicle access from the station. This is nearly identical to a fire station access onto Highway 214 in Woodburn which carries over 15,000 vehicles per day. Pine Street carries less than 1,000 vehicles per day and is forecast to carry less than 4,000 vehicles per day in the future.
2. Approve the construction of the two driveways on either side of the station which allows internal circulation for fire personnel as well as emergency vehicles returning to the station.
3. Provide the fire station sign on both approaches to the fire station of Pine Street. These signs will be placed in accordance with the Manual on Uniform Traffic Control Devices (MUTCD Sign number W11-8).
4. Provide emergency vehicle pre-emption capability in the traffic signal to be installed on Highway 99E at Pine Street.

The proposed location for the fire station offers an excellent site for providing safe and efficient operation for both emergency vehicles and the motoring public.

Sincerely,



Thomas Schwab

cc: Jack Stark - Canby Fire Department

PLEASE RETURN ATTACHMENTS!!!

CANBY PLANNING DEPARTMENT REQUEST FOR COMMENTS

P.O. Box 930, Canby, OR 97013

[503] 266-4021

DATE: June 1, 1995

TO: FIRE, POLICE, CUB, TOM PIERSON/ TODD SCHMIDT, NW NATURAL GAS, MIKE JORDAN, JOHN KELLEY, ROY, STEVE, ODOT

The City has received DR 95-11, an application by Regan Canby Fire District No. 62 for approval to construct a 14,215 square foot 1-story masonry fire station with double loaded 5-bay apparatus bay, administrative offices, living quarters, and meeting space. The site is located south of Highway 99-E on S. Pine Street [Tax Lot 804 of Tax Map 3-1E-33DA].

We would appreciate your reviewing the enclosed application and returning your comments by **June 12, 1995 PLEASE**. The Planning Commission plans to consider this application on June 26, 1995. Please indicate any conditions of approval you may wish the Commission to consider if they approve the application. Thank you.

Comments or Proposed Conditions:

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will **become available** through the development
- Conditions are needed, as indicated
- Adequate public services are **not available** and will not become available

Signature: Jerry Stiger

Date: 6/1

EXHIBIT
5
7 pages

PLEASE RETURN ATTACHMENTS!!!

CANBY PLANNING DEPARTMENT REQUEST FOR COMMENTS

P.O. Box 930, Canby, OR 97013

[503] 266-4021

DATE: June 1, 1995

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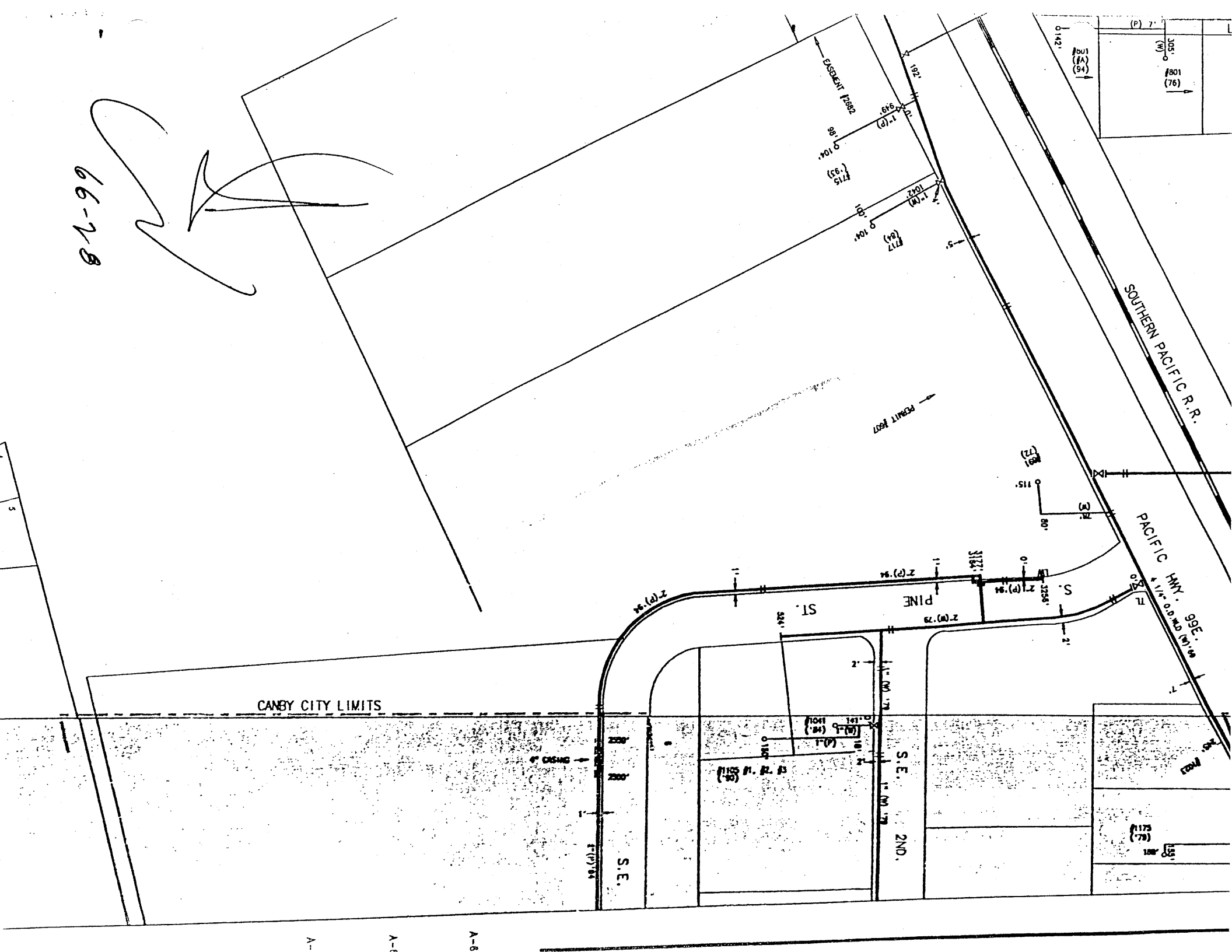
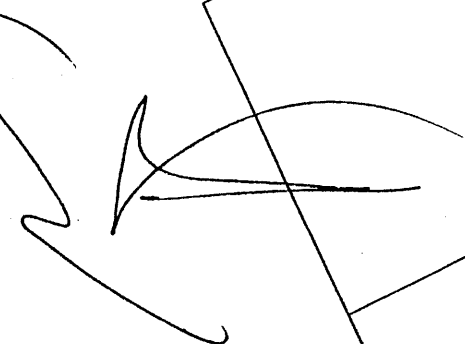
Comments or Proposed Conditions:

Please check one box:

- Adequate Public Services (of your agency) are available
- Adequate Public Services will become available through the development
- Conditions are needed, as indicated
- Adequate public services are not available and will not become available

Signature: Ray F. [Signature] Date: 6/8/95

66-28



CANBY CITY LIMITS

ESSENT 1980

PERMIT 1987

SOUTHERN PACIFIC R.R.

PACIFIC HWY. 99E.
1 1/2' O.D. (1919)

ST. PINE

S.E. 2ND.

S.E.

175 (76)

150

104 (84)

101

109 (72)

0 115'

102 (84)

0 142'

105 (76)

108 (76)

(P) 7'

A-

A-

A-8

PLEASE RETURN ATTACHMENTS!!!

CANBY PLANNING DEPARTMENT REQUEST FOR COMMENTS

P.O. Box 930, Canby, OR 97013

[503] 266-4021

DATE: June 1, 1995

TO: FIRE, POLICE, CUB, TOM PIERSON/ TODD SCHMIDT, NW NATURAL GAS, MIKE JORDAN, JOHN KELLEY, ROY, STEVE, ODOT

The City has received DR 95-11, an application by Regan Canby Fire District No. 62 for approval to construct a 14,215 square foot 1-story masonry fire station with double loaded 5-bay apparatus bay, administrative offices, living quarters, and meeting space. The site is located south of Highway 99-E on S. Pine Street [Tax Lot 804 of Tax Map 3-1E-33DA].

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Comments or Proposed Conditions:

none

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- Adequate public services are **not available** and will not become available

Signature: *S. Nance*

Date: *6-2-95*

PLEASE RETURN ATTACHMENTS!!!

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P.O. Box 930, Canby, OR 97013

[503] 266-4021

DATE: June 1, 1995

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Comments or Proposed Conditions:

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Signature: Tom Pierson Date: 6/5/95

PLEASE RETURN ATTACHMENTS!!!

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P.O. Box 930, Canby, OR 97013

[503] 266-4021

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Comments or Proposed Conditions:

None

Please check one box:

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- Adequate public services are **not available** and will not become available

Signature: *Ken Garbrough*

Date: *6-13-95*

PLEASE RETURN ATTACHMENTS!!!

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P.O. Box 930, Canby, OR 97013

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DATE: June 1, 1995

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Comments or Proposed Conditions:

6" SAN SEWER TO BEHIND PROPERTY LINE W/CLEANOUT
2ND SAN CLEANOUT TO BE LOCATED WITHIN 5' OF FOUNDATION
WOULD SUGGEST CHANGES TO BE MADE IN STORM
SYSTEM PIPE DIA. DRY WELL, CLEANOUT & CATCH BASIN LOCATIONS

Please check one box:

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Signature: Dan Mickelsen

Date: June 9, 1995