

# City of Canby

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## SITE AND DESIGN REVIEW STAFF REPORT

FILE #: DR 16-04

Prepared for the July 25, 2016 Planning Commission Meeting

By Bryan Brown, Planning Director

**LOCATION:** 1400 SE Township Road

**TAX LOT:** 31E34C01817 (Bordered in map below)



**LOT SIZE:** 0.5 acres (21,822 square feet)

**ZONING:** M-1 Light Industrial Zone

**OWNER:** Jeremy Conray (Coffman Excavation) & Rudnick Holdings, LLC (Rudnick Electric Signs, LLC)

**APPLICANT:** Scott Davidson (Davidson Design)

**APPLICATION TYPE:** Site & Design Review (Type III)

**CITY FILE NUMBER:** DR 16-04

## **APPLICANT'S REQUEST:**

The applicant is seeking site and design review approval to construct a 4,896 square foot commercial/manufacturing building on a 0.5 acre property at 1400 SE Township Road. The development proposal is intended to serve Rudnick Electric Signs. The site will consist of an office area, sign fabrication shop, fleet service bay, and mezzanine space above the office area for large format printing. The site development is to include a steel clad building with one truck service bay door in the north elevation away from the public street frontage where a screened fenced in area will accommodate a paved service drive with 3 parking spaces and a similar sized gravel storage area to north with gates and three additional parking spaces are proposed directly off of a common shared paved drive that serves four platted lots in this industrial zone platted subdivision.

## **SECTION I STAFF RECOMMENDATION:**

Based upon the findings in this Staff Report, Staff recommends **APPROVAL** of Site & Design Review DR16-04, subject to conditions presented in this Staff Report.

## **SECTION II APPLICABLE REVIEW CRITERIA:**

### **City of Canby Land Development and Planning Ordinance Chapters:**

- 16.08 General Provisions**
- 16.10 Off-Street Parking and Loading**
- 16.32 M-1 Light Industrial Zone**
- 16.41 Downtown Canby Overlay Zone**
- 16.42 Signs**
- 16.43 Outdoor Lighting Standards**
- 16.46 Access Standards**
- 16.49 Site and Design Review**
- 16.89 Application and Review Procedures**

## **SECTION III REVIEW FOR CONFORMANCE WITH APPLICABLE APPROVAL CRITERIA:**

- 16.08 General Provisions:**
- 16.08.070 Illegally Created Lots**



In no case shall a lot which has been created in violation of state statute or city ordinance be considered as a lot of record for development purposes, until such violation has been legally remedied. (Ord. 740 section 10.3.05(G), 1984)

**Findings:** The tract for this development is Lot 2 of Rustbusters (County Plat #3649). This is a replat of Lot 4 of Township Industrial Park. Staff has determined that the property consists of a legally created lot for land use development purposes.

#### **16.08.090 Sidewalks Requirements**

A. In all commercially zoned areas, the construction of sidewalks and curbs (with appropriate ramps for the handicapped on each corner lot) shall be required as a condition of the issuance of a building permit for new construction or substantial remodeling, where such work is estimated to exceed a valuation of twenty thousand dollars, as determined by the building code. Where multiple permits are issued for construction on the same site, this requirement shall be imposed when the total valuation exceeds twenty thousand dollars in any calendar year.

B. The Planning Commission may impose appropriate sidewalk and curbing requirements as a condition of approving any discretionary application it reviews. (Ord. 740 section 10.3.05(I), 1984)

**Findings:** An existing curb tight 5' wide sidewalk is constructed along the frontage of this lot. This is one foot less than today's sidewalk width requirement but matches the existing sidewalk widths on both sides of this property. The sidewalk requirement is considered to have been met with the existing sidewalk.

#### **16.08.150 Traffic Impact Study (TIS)**

A. Purpose. The purpose of this section of the code is to implement Section 660-012-0045(2)(b) of the State Transportation Planning Rule, which requires the city to adopt a process to apply conditions to development proposals in order to minimize adverse impacts to and protect transportation facilities. This section establishes the standards to determine when a proposal must be reviewed for potential traffic impacts; when a Traffic Impact Study must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities: what information must be included in a Traffic Impact Study; and who is qualified to prepare the Study.

B. Initial Scoping. During the pre-application conference, the city will review existing transportation data to determine whether a proposed development will have impacts on the transportation system. It is the responsibility of the applicant to provide enough detailed information for the city to make a determination. If the city cannot properly evaluate a proposed development's impacts without a more detailed study, a transportation impact study (TIS) will be required to evaluate the adequacy of the

transportation system to serve the proposed development and determine proportionate mitigation of impacts. If a TIS is required, the city will provide the applicant with a “scoping checklist” to be used when preparing the TIS.

C. Determination. Based on information provided by the applicant about the proposed development, the city will determine when a TIS is required and will consider the following when making that determination.

1. Changes in land use designation, zoning designation, or development standard.
2. Changes in use or intensity of use.
3. Projected increase in trip generation.
4. Potential impacts to residential areas and local streets.
5. Potential impacts to priority pedestrian and bicycle routes, including, but not limited to school routes and multimodal street improvements identified in the TSP.
6. Potential impacts to intersection level of service (LOS).

**Findings:** Based on criteria listed in *16.08.150 (C)* above, staff determined that a limited scope TIS would be required for this particular development proposal. DKS Associates assisted staff with providing the applicant an initial scoping checklist with necessary elements to include in the TIS. We negotiated a slightly reduced task list that eliminated formal on-site traffic circulation functionality in order to reduce the cost since the on-site access exists and the site development and traffic generation is limited. The applicant chose to have DKS Associates complete the Traffic Study which was finalized on June 15, 2016. The study indicated that no off-site impacts were necessary to review due to the low additional peak hour traffic generation expected. Site access was determined to be adequate by way of Township Road and the private shared driveway. An on-site sidewalk connects the front door to the public sidewalk. No sight distance issues were present at the driveway connection to the public street.

## **16.10 Off Street Parking**

### **16.10.010 Off-street Parking Required - Exceptions**

A. At the time of establishment of a new structure or use, change in use, or change in use of an existing structure, within any planning district of the city, off-street parking spaces and off-street loading berths shall be as provided in this and following sections, unless greater requirements are otherwise established by the conditional use permit or the site and design review process, based upon clear and objective findings that a greater number of spaces are necessary at that location for protection of public health, safety and welfare. A lesser number of spaces may be permitted by the Planning Commission based on clear

and objective findings that a lesser number of parking spaces will be sufficient to carry out the objective of this section.

**Findings:** The proposed 4,896 square foot commercial building will contain an approximate 2,000 sf sign fabrication shop, a 1200 square foot fleet service area, and nearly 1800 square feet of office type area. Parking is calculated at 1 space per thousand square feet for the fabrication and service area and 2 spaces per thousand square feet for the office type area. This results in a rounded minimum parking requirement of 6 parking spaces. This criterion is met with 3 paved parking spaces delineated on the west side of the building and another 3 spaces along the north end of the building within the enclosed screened paved parking and gravel storage area. The applicant indicates 7 parking spaces have been provided, but staff only identifies 6 paved parking spots on the site-plan.

**16.10.060 Off-street loading facilities**

A. The minimum number of off-street loading berths for commercial and industrial uses is as follows:

SQUARE FEET OF FLOOR AREA	NUMBER OF BERTHS
Less than 5,000	0
5000 – 25,000	1
25,000 – 60,000	2
60,000 and over	3

**Findings:** The proposed building is less than 5000 square feet and therefore is not required to provide a specified loading berth space. This review criterion has been met.

**16.10.070 Parking lots and access**

**16.10.100 Bicycle Parking**

**Findings:** Staff finds that applicant's narrative substantially addresses this criterion. However, we are concerned that the required full 20' wide minimum paved access width required to serve multiple commercially developed lots may not be guaranteed. We find evidence for only a guaranteed shared ingress and egress easement for the full depth of this property to be an 20 foot wide reciprocal ingress and egress easement along the western border of this lot as indicated on the Rustbusters recorded replat of Lot 4, Township Industrial Park plat. This easement in itself is adequate in width for access but the site plan presented for approval indicates that a narrow (approximate 3'

wide landscape strip) is proposed on the west side of the enclosed fenced area which is within the east edge of the designated access easement. This would mean that only 18' 7 ¾" of paved drive exists as proposed. The applicant indicates in their narrative that they share paved access with the adjacent property to the west. This physical shared paved access does exist, but staff has not been presented any legal proof that a corresponding share reciprocal ingress and egress easement is recorded in association with the ownership on the east boundary of the adjacent lot. Without proof of legal access rights to be driving on the neighboring property for shared access between lots a full unobstructed paved 20 foot wide drive is needed on the west side of the applicant's property. Therefore staff recommends that the applicant: 1) demonstrate that a shared reciprocal ingress and access easement exists along the east edge of the adjacent property for the full depth; or, 2) to eliminate the proposed narrow landscape area just west of the enclosed fenced area, increase the paved width of the access on the applicants property to a full 20 foot width, and move the building a minimum of 1' 4 ¼" to the east along with the angled parking spaces so the end of the designated parking stalls do not extend into the 20' wide access easement.

The narrative indicates that 2 bicycle racks are to be provided inside the enclosed fence area just to the north of the building, but their location is not shown on the plan and may be in conflict with the designated parking spaces or electric transformer. A condition of approval will require a revised drawing designating the bike rack location with approval of the final contract approved set of construction drawings.

#### **16.32.010 M-1 Light Industrial Zone**

**Findings:** The zoning of the property is N-1. Section 16.32.010(A) of the M-1 zone lists uses permitted outright in the M-1 zone. Manufacturing and fabricating processes are a permitted outright use along with incidental office and fleet service area uses. The proposed use has been determined to comply with the permitted uses in this zone so this review criterion is met.

#### **16.32.030: Development Standards**

**Findings:** All applicable lot standards, yard setbacks, and building height provisions have been met.

#### **16.42.040 Signs**

**Findings:** The applicant has indicated that a possible wall sign on the south street facing façade of his building is likely at a future time and a multi-tenant pole sign to serve all the business uses occupying the Rustbusters platted area in recognition of the shared access and business visibility desired by each due to their interior location. Common responsibility for such a sign is being worked out amongst the parties. Through the site and design review process, staff recommends that such future pole or possible

monument sign, be limited by a condition of approval of the design review to that allowed in association with a Minor Business Complex in Table 2 of the sign ordinance due to the residential uses directly across from its location on SE Township Road.

#### **16.43 Outdoor Lighting Standards**

**Findings:** The applicant indicates in the narrative that exterior lighting cut sheets have been provided, but staff has been unable to locate them. Detailed lighting inside the building has been indicated but the zoning provisions are only interested in assuring that outdoor light fixture or pole lighting are properly shielded to prevent light trespass into the sky or across property boundaries and that associated mounting height recognizes the light trespass concerns. A condition of approval to provide further information on the location, lumen output, and shielding aspects for any planned outdoor lighting fixtures prior to issuance of a building permit will satisfy conformance with this review criterion.

#### **16.46 Access Limitations on Project Density**

**Findings:** Ingress and egress for the project is onto SE Township Road by means of an existing paved private shared access road. A 20' wide reciprocal share access easement exists on the west boundary of the applicant's property for this purpose. As mentioned previously in this report, a full 20' of guaranteed paved legal shared access is required to meet Canby Fire District emergency access standards and to comply with minimum drive widths for serving commercial properties containing more than 5 parking spaces. CMC Section 16.46.040(B) (2) requires that joint and cross access easements have a minimum width of 20 feet to accommodate two-way travel. The site plan as proposed only provides 18' 7 ¾" wide paved access drive with a legally provided reciprocal cross access easement amongst shared users. A condition of approval has been included in the recommendation to assure a 20' legally guaranteed share paved access drive will exist with this development.

#### **16.49.040 Site and Design Review - Criteria and Standards**

- B. In review of a Type III Site and Design Review Application, the Board shall, in exercising or performing its powers, duties or functions, determine whether there is compliance with the following:
  - 1. The proposed site development, including the site plan, architecture, landscaping and graphic design, is in conformance with the standards of this and other applicable city ordinances insofar as the location, height and appearance of the proposed development are involved; and

2. The proposed design of the development is compatible with the design of other developments in the same general vicinity; and

3. The location, design, size, color and materials of the exterior of all structures and signs are compatible with the proposed development and appropriate to the design character of other structures in the same vicinity.

4. The proposed development incorporates the use of LID best management practices whenever feasible based on site and soil conditions. LID best management practices include, but are not limited to, minimizing impervious surfaces, designing on-site LID storm water management facilities, and retaining native vegetation.

5. The Board shall, in making its determination of compliance with this Ordinance, shall use the matrix in Table 16.49.040 to determine compatibility unless this matrix is superseded by another matrix applicable to a specific zone or zones under this title. An application is considered to be compatible with the standards of Table 16.49.040 if the following conditions are met:

a. The development accumulates a minimum of 60 percent of the total possible number of points from the list of design criteria in Table 16.49.040; and

b. At least 10 percent of the points used to comply with (a) above must be from the list of LID Elements in Table 16.49.040. (Ord. 1338, 2010).

D. In review of a Type III Site and Design Review Application, the Board shall, in exercising or performing its powers, duties or functions, determine whether there is compliance with the INTENT of the design review standards set forth in this ordinance.

**Findings:** A site and design review is required for all new buildings. Compliance with a passing score from the Site Design Review Menu in Table 16.49.040 must be shown and with the landscaping. Under provisions noted in 16.49.040(D) above, the Board can determine if the project meets the intent of design review standards under a Type III review.

The applicant provided a response to Table 16.49.040. A generous interpretation of the development's conformance with the necessary point total indicates the project meets the minimum standard. It is not clear what part of the paved parking area is intended to utilize pervious pavement which is necessary to obtain the necessary point total. As a condition of approval, the applicant shall indicate with final construction plan approval that 10 to 50% of the new paved parking area planned to utilize pervious pavement as indicated in the Design Review Point Matrix.

Staff has agreed that one-half of the enclosed fence yard can qualify as qualify as storage area for sign staging and supplies thus allowing a gravel surface.

#### **16.49.065 Bicycle and pedestrian facilities**



Developments coming under design review shall meet standards listed in this section.

**Findings:** Two bicycle racks are proposed near the building in the enclosed parking and service drive area. A condition of approval requires that the final approved construction plans designate the exact location for the two bike racks. An internal sidewalk is proposed from the building to the public street. With the condition of approval for the bike rack location designation this criterion has been met.

#### **16.49.070 Landscaping provisions, Authority and intent**

The purpose of this section is to establish standards for landscaping within the City of Canby in order to enhance the environmental and aesthetic quality of the city

#### **16.49.080 General provisions for landscaping**

**Findings:** The landscaping calculation work sheet which is part of the Site and Design Review application form was completed by the applicant. The industrial zoned property is subject to an overall landscaping area equal to 15% of the total lot area. The calculations show compliance with the overall landscaping area at about 17% of the site with 539 square feet to spare from the minimum. The landscaping is distributed in a manner to provide in excess of 15% of the total within 10 feet of the parking and drive areas provided as well. The proposed landscaping plant material is considered suitable and will be maintained by having a hose bib generally within 150' of the site landscaping as allowed rather than an irrigation system on this small site. Bark mulch is to cover the entire non-hard surface area except where groundcover, shrubs, and trees are to be planted. Reduction in the use of grass is encouraged to reduce water use and fertilizer use and potential runoff into the ground water or surface rivers. The landscaping requirements are considered to have been met.

### **16.89 Application and Review Procedures**

#### **16.89.020 Description and Summary of Processes**

All land use and development applications shall be decided by using the procedures contained in this Chapter. Specific procedures for each type of permit are contained in Sections 16.89.030 through 16.89.060. The procedure type assigned to each permit governs the decision-making process for that permit. Additional requirements may be found in the individual chapters governing each permit type. The four types of procedure are described below. Table 16.89.020 lists the City's land use and development applications and their required procedures.

C. Type III Procedure (Quasi-Judicial/Legislative). Type III decisions are made by the Planning Commission after a public hearing, with appeals reviewed by the City Council. Type III procedures generally use discretionary approval criteria.

**Finding:** The proposed project is subject to a Type III Site and Design Review procedure. A Type III decision is the only option available for a new building over 5,000 square feet in size when not located in the Downtown Canby Overlay District. This type of review is subject to Planning Commission review and decision.

#### **16.89.050 Type III Decision**

**Findings:** Requirements under this section are included in the application materials. A Pre-application was held on March 22, 2016, a neighborhood meeting was conducted on July 7, 2016 on site. Procedural application requirements which are a responsibility of the applicant were met. Staff notified surrounding property owners within a 500 foot radius of the public hearing on July 8, 2016, with a notice sign placed on the property on July 11, 2016 and a legal notice of the public hearing to be held in the Canby Herald on July 20, 2016. City required procedural notifications were met.

**Comments:** Comments were solicited from the public, City departments, and applicable reviewing agencies. Summary of comments are included in the staff report, and complete agency and public comments are part of the file. All comments from citizens and agencies received to date are attached to this report while any additional will be presented to the Planning Commission at the public hearing.

#### **Public Comments:**

No public comments were received at the time this staff report was written.

#### **Agency Comments:**

Canby Fire District indicated by email that two fire hydrants are located within the prescribed distance to handle a non-sprinkler system building. No additional utility agency comments were received other than that provided in the minutes of the pre-application meeting. Adequate public services are available to serve this site.

The City Engineer has recommended that the broken portion of the concrete driveway be removed and replaced in association with the proposed development. Although the broken drive approach will serve this development, its deterioration has resulted from use by others that share the approach. Staff's understanding is that there is not currently a shared maintenance agreement in affect for the shared drive. Owners are currently discussing the need for such. Requiring replacement of the broken portion of the driveway approach of this applicant would only be appropriate if it is clearly within the bounds of the applicant's property but may be best to be a future maintenance item to be budgeted by all the users when an agreement has been put in place for its care.

## **SECTION IV CONCLUSION/RECOMMENDATION**

Staff concludes that the use is in conformance with the City's Comprehensive Plan and the Zoning Ordinance. Additionally, the relevant site and design standards are met or can be met with conditions. Minimum acceptable design review compatibility scores are met, and the site can accommodate the proposed use with all public service and utility services available or to be made available through future improvements to adequately serve the site.

Staff recommends approval of DR 16-04 subject to meeting the following conditions of approval.

## **SECTION V CONDITIONS OF APPROVAL**

Approval of this application is based on submitted application materials. Approval is strictly limited to the submitted proposal and is not extended to any other development of the property. Any modification of development plans not in conformance with the approval of application file DR16-04, including all conditions of approval, shall first require an approved modification in conformance with the relevant sections of the Canby Municipal Code.

### **Conditions Unique to this Proposal**

1. A revised site plan shall designate the specific location planned for bike racks with approval of the final construction drawings.
2. The future planned multi-tenant pole or monument sign shall be limited to the size and height standards applicable to a "Minor Business Complex" as indicated in Table 2 of the sign ordinance in recognition of the residential uses directly across SE Township Road from the sign location and shall secure required sign permit approvals separate from this application.
3. The applicant shall provide documentation indicating the location, lumen output, and shielding aspects for any planned outdoor lighting fixtures or parking area lighting with approval of the final construction drawings.
4. The applicant shall demonstrate that a shared reciprocal ingress and access easement for his use exists along the east edge of the adjacent property for the full depth of the applicant's lot; or, 2) eliminate all encroachments indicated within the designated 20 foot wide access easement – including all landscape areas and the rear portion of the designated parking stalls as dimensioned by ordinance and increase the paved width of the access on the applicants property to a full 20 foot width. (This appears to be possible by moving the building and angle parking to the east within the designated 12 foot setback currently provided along the east boundary of the property).
5. Demonstrate on a revised site plan where 10 to 50% of the new paved parking area shall utilize previous pavement.

## **Procedural Conditions**

### **Prior to Issuance of building permits, the following must be completed:**

6. The design engineer shall submit to the City of Canby for review and approval at the time of final construction plan approval a storm drainage analysis and report applicable to the defined development area detailing how storm water disposal from both the building and the parking areas is being handled with calculations to justify the sizing of the proposed soakage or detention facilities. Any drainage plan analysis shall meet shall conform to the Clean Water Services storm drainage design standards as indicated in Chapter 4 of the Canby Public Works design standards dated June 2012. All on-site drywells shall be ruled authorized by the Department of Environmental Quality (DEQ). A copy of the registration shall be submitted to the City prior to any storm drainage construction.
7. A sediment and Erosion Control Permits will be required from the City prior to commencing site work. They are usually applied for with a building permit release letter when System Development Fees are due and payable.
8. Prior to the issuance of a building permit, the installation of public utilities, or any other site work other than rough site grading, construction plans must be approved and signed by the City and all other utility/service providers. A Pre-Construction Conference with sign-off on all final construction plans is required. The applicant may submit the civil construction drawings separate from the building permit submittal for final preconstruction conference sign-off approval. The design, location, and planned installation of all roadway improvements and utilities including but not limited to water, electric, sanitary sewer, natural gas, telephone, storm water, cable television, and emergency service provisions is subject to approval by the appropriate utility/service provider. The City of Canby's preconstruction process procedures shall be followed. Construction plans shall be designed and stamped by a Professional Engineer registered in the State of Oregon.
9. Clackamas County will provide structural, mechanical, grading, and review of Fire & Life Safety, Plumbing, and Electrical permits for this project. Fire & Life Safety approval must be obtained from Canby Fire District prior to issuance of a City building permit.

## **Section VI      Attachments/Exhibits:**

1. Applicant's Application & Narrative Submittal
2. Applicant's Proposed Site Plan/Elevations/Details
3. Traffic Study
4. Rustbusters Plat
5. Agency Comments
6. Neighborhood Mtg. Minutes & Attendance Sign-in
7. Pre-application Conference Summary



City of Canby  
Planning Department  
111 NW 2<sup>nd</sup> Avenue  
PO Box 930  
Canby, OR 97013  
(503) 266-7001

# LAND USE APPLICATION

## SITE AND DESIGN REVIEW

### General Type III

#### APPLICANT INFORMATION: (Check ONE box below for designated contact person regarding this application)

☐ Applicant Name: Scott Davidson Phone: 24-315-5362  
Address: 7121 SE Carlton St. Email: scottdavidsondesign@gmail.com  
City/State: Portland OR Zip: 97206

☒ Representative Name: Nate Parrish / NPP Construction Phone: 503-789-8534  
Address: 9498 SW Barber blvd. Email: nateparrish@sterling.net  
City/State: PORTLAND, OR. Zip: 97206

☐ Property Owner Name: Jeremy Conway Phone: 503-710-0928  
Signature: [Signature] Coffman Excavation  
Address: 13014 Clackamas River dr Email: Jeremy@coffmanteam.com  
City/State: Oregon City, OR. Zip: 97045

☐ Property Owner Name: Rudnick Holdings LLC - Phone: 503-655-2610  
Signature: \_\_\_\_\_  
Address: 11401 S Finnegans WAY Email: steve@rudnickelectricians.com  
City/State: Oregon City, OR Zip: 97051

NOTE: Property owners or contract purchasers are required to authorize the filing of this application and must sign above

- 1 All property owners represent they have full legal capacity to and hereby do authorize the filing of this application and certify that the information and exhibits herewith submitted are true and correct.
- 2 All property owners understand that they must meet all applicable Canby Municipal Code (CMC) regulations, including but not limited to CMC Chapter 16.49 Site and Design Review standards.
- 3 All property owners hereby grant consent to the City of Canby and its officers, agents, employees, and/or independent contractors to enter the property identified herein to conduct any and all inspections that are considered appropriate by the City to process this application.

#### PROPERTY & PROJECT INFORMATION:

1400 SE Township RD 1/2 Acre 31E34C01817  
Street Address or Location of Subject Property Total Size of Property Assessor Tax Lot Numbers  
None M-1 Light Industrial  
Existing Use, Structures, Other Improvements on Site Zoning Comp Plan Designation

New Development - Service Headquarters / Fabrication Shop  
Describe the Proposed Development or Use of Subject Property

STAFF USE ONLY				
<u>DR16-04</u>	<u>6/24/16</u>	<u>[Signature]</u>		
FILE #	DATE RECEIVED	RECEIVED BY	RECEIPT #	DATE APP COMPLETE

## SITE AND DESIGN REVIEW APPLICATION – TYPE III–INSTRUCTIONS

*All required application submittals detailed below must also be submitted in electronic format on a CD, flash drive or via email. Required application submittals include the following:*

Applicant City  
Check Check

- ☒ ☐ One (1) copy of this application packet. The City may request further information at any time before deeming the application complete.
- ☐ ☐ Payment of appropriate fees – cash or check only. Refer to the city's Master Fee Schedule for current fees. Checks should be made out to the *City of Canby*.
- ☐ ☐ Mailing labels (1" x 2-5/8") for all property owners and all residents within 500 feet of the subject property. **If the address of a property owner is different from the address of a site, a label for each unit on the site must also be prepared and addressed to "occupant."** A list of property owners may be obtained from a title insurance company or from the County Assessor.
- ☒ ☐ One (1) copy of a written, narrative statement describing the proposed development and detailing how it conforms with the Municipal Code and to the approval criteria, including the applicable Design Review Matrix, and availability and adequacy of public facilities and services. Ask staff for applicable Municipal Code chapters and approval criteria.  
Applicable Code Criteria for this application includes:  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_
- ☒ ☐ Three (3) copies of a Traffic Impact Study (TIS), conducted or reviewed by a traffic engineer that is contracted by the City and paid for by the applicant (payment must be received by the City before the traffic engineer will conduct or review a traffic impact study.  
Ask staff to determine if a TIS is required. *6-13-16 waiting to receive copy*
- ? - ☒ ☐ One (1) copy in written format of the minutes of the neighborhood meeting as required by Municipal Code 16.89.020 and 16.89.070. The minutes shall include the date of the meeting and a list of attendees. *have pre-application minutes*
- ☒ ☐ One (1) copy in written format of the minutes of the pre-application meeting
- ☒ ☐ One copy of either the recorded plat or the recorded deeds or land sales contracts that demonstrates how and when legal property lines were established and where the boundaries of the legal lot(s) of record are located. If the property is a lot or parcel created by plat, a copy of the recorded plat may be obtained from the Clackamas County Surveyor's office. If the property is a legal lot of record created by recorded deed or land sales contract at a time when it was legal to configure property lines by deed or contract, then those recorded deeds may be obtained from the Clackamas County Office of the Clerk, or a Title Company can also assist you in researching and obtaining deeds.
- NA* ☒ ☐ If the development is located in a Hazard ("H") Overlay Zone, submit one (1) copy of an affidavit signed by a licensed professional engineer that the proposed development will not result in significant impacts to fish, wildlife and open space resources of the community. If major site grading is proposed, or removal of any trees having trunks greater than six inches in diameter is proposed, then submit one (1) copy of a grading plan and/or tree-cutting plan.



Applicant City  
Check Check  
☒ ☐

Ten (10) paper copies of the proposed plans, printed to scale no smaller than 1"=50'. The plans shall include the following information:

- ☐ Vicinity Map. Vicinity map at a scale of 1"=400' showing the relationship of the project site to the existing street or road pattern.
- ☐ Site Plan-the following general information shall be included on the site plan:
  - ☐ Date, north arrow, and scale of drawing;
  - ☐ Name and address of the developer, engineer, architect, or other individual(s) who prepared the site plan;
  - ☐ Property lines (legal lot of record boundaries);
  - ☐ Location, width, and names of all existing or planned streets, other public ways, and easements within or adjacent to the property, and other important features;
  - ☐ Location of all jurisdictional wetlands or watercourses on or abutting the property;
  - ☐ Finished grading contour lines of site and abutting public ways;
  - ☐ Location of all existing structures, and whether or not they are to be retained with the proposed development;
  - ☐ Layout of all proposed structures, such as buildings, fences, signs, solid waste collection containers, mailboxes, exterior storage areas, and exterior mechanical and utility equipment;
  - ☐ Location of all proposed hardscape, including driveways, parking lots, compact cars and handicapped spaces, loading areas, bicycle paths, bicycle parking, sidewalks, and pedestrian ways;
  - ☐ Callouts to identify dimensions and distances between structures and other significant features, including property lines, yards and setbacks, building area, building height, lot area, impervious surface area, lot densities and parking areas;
  - ☐ Location of vision clearance areas at all proposed driveways and streets.
- ☐ Landscape Plan, with the following general information:
  - ☐ Layout and dimensions of all proposed areas of landscaping;
  - ☐ Proposed irrigation system;
  - ☐ Types, sizes, and location of all plants to be used in the landscaping (can be a "palette" of possible plants to be used in specific areas for landscaping);
  - ☐ Identification of any non-vegetative ground cover proposed, and dimensions of non-vegetative landscaped areas;
  - ☐ Location and description of all existing trees on-site, and identification of each tree proposed for preservation and each tree proposed for removal;
  - ☐ Location and description of all existing street trees in the street right-of-way abutting the property, and identification of each street tree proposed for preservation and each tree proposed for removal.
  - ☐ Elevations Plan
    - The following general information shall be included on the elevations plan:
    - ☐ Profile elevations of all buildings and other proposed structures;
    - ☐ Profile of proposed screening for garbage containers and exterior storage areas;
    - ☐ Profile of proposed fencing.
- ☐ Sign Plan.
  - ☐ Location and profile drawings of all proposed exterior signage.
- ☐ Color and Materials Plan.
  - ☐ Colors and materials proposed for all buildings and other significant structures.

☐ ☐ One (1) copy of a completed landscaping calculation form (see page 5)

☐ ☐ One (1) copy of a completed Design Review Matrix (see page 6)

# SITE AND DESIGN REVIEW APPLICATION: LANDSCAPING CALCULATION FORM

## Site Areas

1. Building area	4000 sq/ft	- Square footage of building footprints
2. Parking/hardscape	14010 sq/ft	- Square footage of all sidewalks, parking, & maneuvering areas
3. Landscaped area	3812 sq/ft	- Square footage of all landscaped areas
4. Total developed area	21822 sq/ft	- Add lines 1, 2 and 3
5. Undeveloped area	0	- Square footage of any part of the site to be left undeveloped.
6. Total site area	21822 sq/ft	- Total square footage of site

## Required Site Landscaping (Code 16.49.080)

7. Percent of landscaping required in Zoning District	15%	- Fill in the Appropriate Percentage: R-1, R-1.5, R-2 Zones: 30%; C-2, C-M, C-R, M-1, M-2 Zones: 15%; C-1 Zone: 7.5%
8. Required minimum square footage of landscaping	3273 #	- Multiply line 4 and line 7
9. Proposed square footage of landscaping	3350 #	- Fill in value from line 3

## Required Landscaping within a Parking Lot (Code 16.49.120(4))

Note: This section and the next apply only to projects with more than 10 parking spaces or 3,500 square feet of parking area

10. Zone	M-1	- Fill in the Appropriate Zone and Percentage: C-1 Zone: 5%; Core Commercial sub-area of the Downtown Canby Overlay: 10%, except for parking lots with 10 or more spaces and two or more drive aisles: 50 square feet per parking space; All other zones: 15%.
11. Percent of required landscaping	15%	
12. Area of parking lot & hardscape	14010 #	- Fill in area of parking and maneuvering areas plus all paved surface within ten (10) feet of those areas.
13. Number of vehicle parking spaces		- For Core Commercial sub-area in the Downtown Canby Overlay only, fill in the total # of parking spaces on-site.
14. Required square footage of landscaping within 10 feet of parking lot	2102 #	- Multiply area of parking lot (line 12) by percent of required landscaping (line 11) -OR- for the CC sub-area in the Downtown Canby Overlay multiply line 13 by 50 square feet.
15. Proposed square footage of Landscaping within 10 feet of parking lot	2350 #	- Calculate the amount of landscaping proposed within 10 feet of all parking and maneuvering areas.

## Parking Lot Tree Calculation

16. Number of parking spaces	7	- Total number of vehicle parking spaces
17. Area of parking lot & hardscape	14010 #	- Area from line 12
18. Number of parking spaces (line 16) divided by 8	1	- Round up to the nearest whole number
19. Area of parking lot area (line 17) divided by 2,800	5	- Round up to the nearest whole number
20. Number of required trees in parking lot	5	- Fill in the larger of row 18 and row 19
21. Number of trees provided within 10 feet of parking lot	5	- Fill in the number of proposed trees within 10 feet of parking and maneuvering areas.



## SITE AND DESIGN REVIEW APPLICATION: DESIGN REVIEW MATRIX

**Applicants:** Please circle the applicable point column to your project and compute the total and percentages at the end of the table.

**Table 16.49.040 Site Design Review Menu**

As part of Site and Design Review, the following menu shall be used as part of the review. In order to "pass" this table 60% of total possible points shall be earned, 10% of the total possible points must be from LID elements

Design Criteria	Possible Points				
Parking	0	1	2	3	4
Screening of parking and/or loading facilities from public right-of-way	Not screened	Partially screened	Fully screened	-	-
Parking lot lighting provided	No	Yes	-	-	-
Parking location (behind building is best)	Front	Side	Behind	-	-
Number of parking spaces provided (% of minimum required)	>120%	101-120%	100%	-	-
Screening of Storage Areas and Utility Boxes	0	1	2	3	4
Trash storage is screened from view by solid wood fence, masonry wall or landscaping.	No	Yes	-	-	-
Trash storage is located away from adjacent property lines.	0 - 10 feet from adjacent property	11 - 25 feet from adjacent property	>25 feet from adjacent property	-	-
Utility equipment, including rooftop equipment, is screened from view.	Not screened	Partially screened	Fully screened	-	-
Access	0	1	2	3	4
Distance of access to nearest intersection.	≤70 feet	71 - 100 feet	>100 feet	-	-
Pedestrian walkways from public street/sidewalks to building entrances.	One entrance connected.	-	Walkways connecting all public streets/sidewalks to building entrances.	-	-
Pedestrian walkways from parking lot to building entrance.	No walkways	Walkway next to building only	Walkways connecting all parking areas to building entrances	-	-

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Tree Retention	0	1	2	3	4
Design Criteria	Possible Points				
Percentage of trees retained	<10%	10-50%	51-75%	>75%	No existing trees - adding
Replacement of trees removed	<50%	≥50%	-	-	-
Signs	0	1	2	3	4
Dimensional size of sign (% of maximum permitted)	>75%	50-75%	<50%	-	-
Similarity of sign color to building color	Not similar	Somewhat similar	Similar	-	-
Pole sign used	Yes	No	-	-	-
Building Appearance	0	1	2	3	4
Style (similar to surroundings)	Not similar	Somewhat similar (1 or 2 points possible depending on level of similarity)		-	-
Color (subdued and similar to surroundings is better)	Neither	Similar or subdued	Both	-	-
Material (concrete, wood and brick are best)	Either 1 or 2 points may assigned at the discretion of the Site and Design Review Board				
Size of building (smaller is better)	>20,000 square feet	≤20,000 square feet	-	-	-
Provision of public art (i.e. murals, statues, fountains, decorative bike racks, etc.)	No	-	-	-	Yes
Landscaping	0	1	2	3	4
Number of non-required trees provided	-	At least one tree per 500 square feet of landscaping.	-	-	-
Amount of grass (less grass is better) (% of total landscaped area)	>50%	25-50%	<25%	-	-
Low Impact Development (LID)	0	1	2	3	4
Use of pervious paving materials (% of total paved area)	<10%	-	10-50%	51-75%	>75%
Provision of park or open space area	None	-	Open space (Generally not for public use)	-	Park (public or privately owned for public use)

18

2      3      4

Design Criteria	Possible Points				
Use of drought tolerant species in landscaping (% of total plants)	<25% drought tolerant	-	25-50% drought tolerant	51-75% drought tolerant	>75% drought tolerant
Provision of additional interior parking lot landscaping (% of minimum required)	100%	101-110%	111-120%	>120%	-
Provision of an eco-roof or rooftop garden (% of total roof area)	<10%	-	-	10-50%	>50%
Parking integrated within building footprint (below-grade, structured parking, or tuck-under parking) (% of total on-site parking)	<10%	-	-	10-50%	>50%
Disconnecting downspouts from city stormwater facilities	None	Some downspouts disconnected	All downspouts disconnected	-	-
Shared parking with adjacent uses or public parking structure (% of total required parking spaces)	None	<50%	≥50%	-	-
Provision of rain gardens/bioretention areas for stormwater runoff (% of total landscaped area)	None	-	10-50%	51-75%	>75%
Total Possible Points = 71, 60%=42.6 points, 10%=7.1 points					

metal roof =  
Clean rain into  
bio-retention  
area

13

Total Points Earned: 48 (42.6 points required for 60%)

Total LID Points Earned: \_\_\_\_\_ (7.1 required for 10%)

## SITE AND DESIGN REVIEW – TYPE III: APPLICATION PROCESS

1. Prior to submitting an application, all applicants are encouraged to request a pre-application meeting with the City -or- the Planning Director may determine that a pre-application meeting is required prior to submitting an application. To schedule a pre-application meeting, an applicant must submit a completed pre-application form and set of preliminary plans to the City Planner, and after receiving the Planner's initials, must then make and take (3) copies of the pre-application materials to the Canby Public Works Department to schedule the pre-application meeting. The amount of the fee for a pre-application meeting is based on whether the application involves a public hearing or not.
2. Prior to submitting an application, applicants may be required to hold a neighborhood meeting with surrounding property owners and any recognized neighborhood association representative, pursuant to the procedures described in Canby Municipal Code Section 16.89.070. In certain situations, the Planning Director may waive the neighborhood meeting requirement.
3. At the time an application is submitted to the City, payment of all required application processing fees is required. An application will not be accepted without payment of fees. City Staff can provide you with information concerning application fees.
4. Staff will check the application, making sure that it is complete and all fees are paid. Copies of the application materials are routed to various City/State/County departments, as applicable, for their comments. The application is reviewed for completeness; the City Planner will accept or return the application with a written list of omissions within thirty (30) calendar days of the submittal.
5. Staff investigates the application, writes a staff report, issues public notice, notifies surrounding property owners, and makes all facts relating to the request available to the Planning Commission and all interested parties.
6. Prior to the public hearing, the City will prepare notice materials for posting on the subject property. This material must be posted **by the applicant** at least ten (10) days before the public hearing.
7. The staff report will be available to all interested parties seven (7) days prior to the hearing.
8. The Planning Commission holds a public hearing. The staff report is presented to the Commission. Testimony is presented by the applicant, proponents and opponents, followed by rebuttal from the applicant.
9. The Commission then issues findings of fact which support approval, modification, or denial of the application. A decision may be appealed to the City Council.
10. If an approval or a denial is appealed, City Council holds a public hearing. The staff report is presented and testimony taken, as at the original hearing(s). Unless the City Council decides to hear the appeal de novo, only testimony regarding items already in the record is permitted, and no new information may be entered. In the case of an appeal, the Council may affirm, revise or reverse the action of the Planning Commission in all or in part. The Council may also remand the matter back to the hearing body for further consideration.
11. Prior to construction of the project, a preconstruction meeting is held with the City and all applicable utility and service providers. If required, this meeting must be held and approval of Plan set by all agencies, and payment of Canby System Development Charge (SDC) and construction excise tax to the City before issuance of any building permits for the project(s) by Clackamas County.



## **SITE AND DESIGN REVIEW – TYPE III: REVIEW CRITERIA (*Code 16.49.040*)**

1. The Planning Commission shall, in exercising or performing its powers, duties or functions, determine whether there is compliance with the following A through D, and with Criteria 4, 5, and 6 below:
  - A. The proposed site development, including the site plan, architecture, landscaping and graphic design, is in conformance with the standards of this and other applicable City ordinances insofar as the location, height and appearance of the proposed development are involved; and
  - B. The proposed design of the development is compatible with the design of other developments in the same general vicinity; and
  - C. The location, design, size, color and materials of the exterior of all structures and signs are compatible with the proposed development and appropriate to the design character of other structures in the same vicinity; and
  - D. The Planning Commission shall, in making its determination of compliance with subsections B and C above, use the applicable matrix [*pages 8-12*] to determine “compatibility”.
2. The Planning Commission shall, in making its determination of compliance with the above requirements, be guided by the objectives and standards set forth in this section. It must be demonstrated that all required public facilities and services are available, or will become available through the development, to adequately meet the needs of the proposed development. If the site and design review plan includes utility facilities or public utility facility, then the City Planner shall determine whether those aspects of the proposed plan comply with applicable standards.
3. The Planning Commission shall, in making its determination of compliance with the requirements set forth, consider the effect of its action on the availability and cost of needed housing. The Planning Commission shall not use the requirements of this section to exclude needed housing types. However, consideration of these factors shall not prevent the Planning Commission from imposing conditions of approval necessary to meet the requirements of this section. The costs of such conditions shall not unduly increase the cost of housing beyond the minimum necessary to achieve the purposes of this ordinance.
4. As part of the site and design review, the property owner may apply for approval to cut trees in addition to those allowed in Chapter 12.32, the City Tree Ordinance. The granting or denial of said application will be based on the criteria in Chapter 12.32. The cutting of trees does not in and of itself constitute change in the appearance of the property which would necessitate application for site and design review.

## Site & Design Review Project Narrative for

# New Commercial Building for Rudnick Electric Signs, LLC

**Applicant:**

Scott Davidson  
Scott Davidson Design @ gmail.com  
7121 SE Carlton ST  
Portland, OR 97206  
(214)315-5302

**Owner:**

Rudnick Holdings, LLC  
11401 S Finnegans Way  
Oregon City, OR 97045  
(503)655-2610

**Property Address:**

1400 Township RD Canby, OR 97013

**Legal & Assessor's Map:**

Assessor Map: 31E34C Tax lot 01817  
Located in the SW corner of tax map 86 -02 on Clack County assessor map(s)  
City of Canby, Clackamas County, Oregon

**Lot Area:**

½ acre

**Zoning:**

M-1

**Architectural & Landscape Consultant:**

Scott Davidson Design  
7121 SE Carlton ST  
Portland, OR 97206  
(214)315-5302

**Civil Engineering Consultant:**

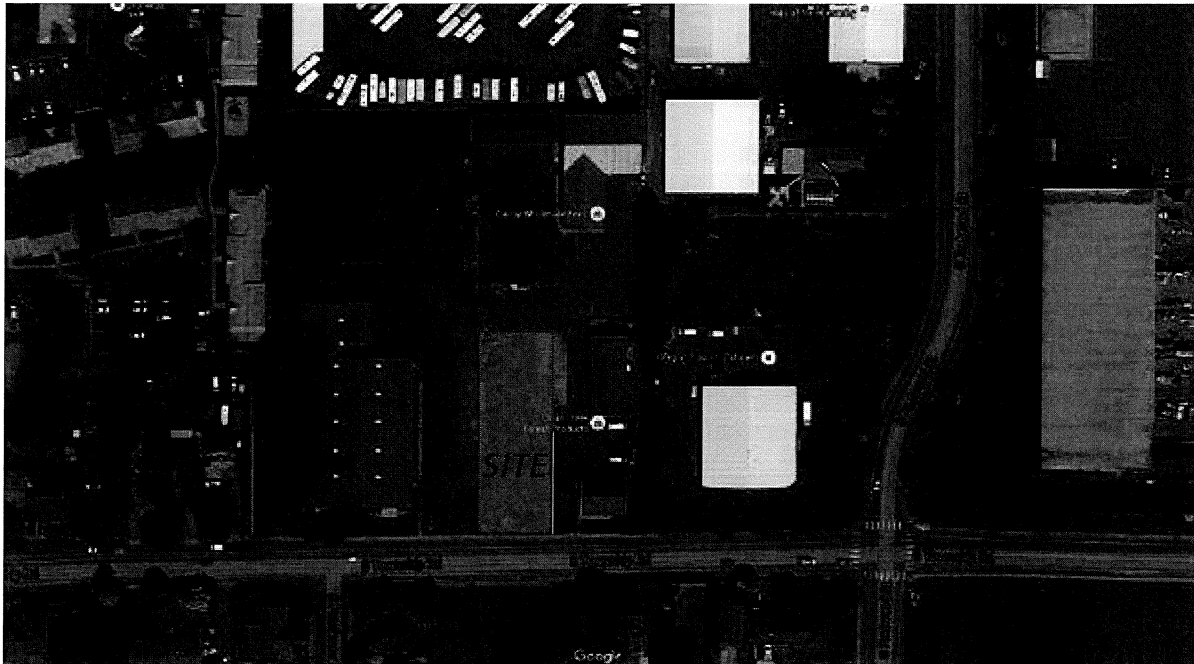
Paul Tibbot, P.E.  
Consulting Engineers  
1500 SW 11<sup>th</sup> Ave  
SUITE 1205  
Portland, OR 97021  
(503)310-9225

**Property Description:**

Rudnick Holdings LLC will be sole occupant on ½ acre lot located on SE Township Road. The site is bordered by the following businesses: Canby Wholesale Feed & to the North / Oregon Custom Cabinets to the East. This location is in the Light Industrial in surrounding directions from the North, South, East, West. There are no existing structures on property in question, new building is proposed to be built to meet all standards as needed in this zoning area (Light Industrial). Proposed building will be a steel clad pole frame building. In proposed building there will be (1) bay door 16'0"H x 24'0"L painted white metal material, (1) office 23'03"W x 40'0"L with one standard half bath located on second floor, (1) fabrication/shop area 50'0"W x 80'0"L with secondary half bath located on first floor, (1) electrical panel box located on first floor.

The site is located in the M-1 Light Industrial area. This will be commercial/manufacturing located on Township RD with conditional uses for commercial recreation uses. The property is a rectangle shape with primary street frontage facing South to Township Road. The site is relatively flat, and undeveloped portion of the site is mostly covered by grass and a larger boulder (rock). The southern portion street front is covered by PRW sidewalk and PWR road, to the west, north, & east elevations are covered by paved asphalt by surrounding commercial businesses.

Directly north of the project site is A.C.T. Mini Storage, to the northeast is Canby Wholesale Feed LLC. To the west is Hot Off The Press Inc. To the East is Clear Creek Forest Products, to the south across S. Township Rd. is a residential neighborhood.



**Proposed Development:**

The Applicant proposes to develop a 4,920 square foot building with the dimensions of 50 feet x 80 feet with associated parking and landscaping areas. Building will be constructed of metal exterior siding with a metal roof. The building interior will be divided into 2 main spaces. A 50' X 40' fabrication shop, a 40' X 25' bay for fleet parking / service and enclosed offices at the northwest corner of the building. Offices will have mezzanine space above for large format printing. On the proposed buildings north side there will be an enclosed parking lot for sign staging, additional parking and will be secured with a 100' X 78' chain link mesh fence with two non-automated locking gates.

**Utility and Service Requirements:**

Water service: A single water meter will supply domestic water to the proposed building. Water will be needed for restrooms, break room and exterior building spiggots. Spiggots will be used for landscape irrigation.

Fire Suppression: A fire hydrant is located less than 50' from the southwest corner of the project site along S. Township Rd. and will provide fire suppression water to the proposed building. The building will not be constructed with a fire sprinkler system.

Sanitary sewer: The offices will have water service while the fabrication shop will not. Wastewater will be typical of commercial office space. One private sewer lateral will be provided to plumb the office space into. The sewer lateral will drain into a public sanitary sewer stub out at the sidewalk / south property line.

Electrical: A single electrical meter will be used for the building.

Natural Gas: The building will not use natural gas.

Phone / cable: Telephone and cable will be needed for each office and mezzanine.

Storm drainage: Storm water from the new building and new pavement surfaces will be managed through construction of a storm drain infiltration basin at the northwest corner of the enclosed park lot and a 5' landscaping swale along the south property line.

Garbage: A garbage and recycling space that is screened behind a 6' chain link fence with privacy slats will be provided inside the northwest corner of the enclosed parking lot.

US Mail: A new mail box unit will be added to the existing mailbox location at the southeast corner of the site along S. Township Rd. or as directed by the U.S. Postal Service.

## **Municipal Code Conformity Title 16**

The following text includes all applicable sections of the current City of Canby Title 16 Planning and Zoning Code, followed by a written statement in *highlighted Italic text* explaining how the proposed project conforms to the given requirement.

### **16.10 OFF-STREET PARKING AND LOADING**

#### **16.10.050 Parking standards designated**

Off-street Parking Provisions – The parking standards identified in Table 16.10.050 are the minimum standards for off-street vehicle parking in the City of Canby. The standards below apply to this development.

<b>Industrial:</b>	
b. Warehousing	2.00 spaces per 1,000 gross square feet of office space, plus 1.00 space per 1,000 gross square feet of non-office warehousing space. Minimum of 5 parking spaces overall

*The proposed building will have a combination of office space and non-office shop space. Although the site is zoned M-1 the listed use that most closely resembles the proposed building is an industrial warehousing use with office space. The parking requirements are as follows:*

*(office / mezzanine)*

*Office space: 2.0 spaces per 1,000 s.f. x 1,840 s.f. = 3.68 stalls required.*

*Shop space: 1.0 space per 1,000 s.f. x 2,791 s.f. = 2.79 stalls required*

*3.68 stalls + 2.79 stalls = 6.47 stalls required. => 6 stalls required*

*7 parking stalls are proposed, exceeding the minimum requirement.*

#### **16.10.060 Off-Street loading facilities**

A. The minimum number of off-street loading berth for commercial and industrial uses is as follows:

SQUARE FEET OF FLOOR AREA	NUMBER OF BERTHS
Less than 5,000	0
5000 – 25,000	1

*The proposed building is 4,920 s.f., therefore (0) loading berth is required per the table above. The project proposes one loading berth and therefore meets the requirement.*

B. Loading berths shall conform to the following minimum size specifications:

1. Commercial Uses – 13' x 35'
3. Berths shall have an unobstructed minimum height of 14 feet.

*None Required. The requirements of this section are met.*

C. Required loading areas shall be screened from public view, from public streets, and adjacent properties by means of sight-site obscuring landscaping, walls or other means, as approved through the site and design review process.

*None Required.  
The requirements of this section are met.*

#### **16.10.070 Parking lots and access.**

A. Parking Lots. A parking lot, whether as accessory or principal use, intended for the parking of automobiles or trucks, shall comply with the following:

1. Parking lot design shall comply with the dimensional standards set forth in Figure 1 of this section.
2. Parking stalls of eight (8) feet in width and sixteen (16) feet in length for compact vehicles may comprise up to a maximum of thirty (30) percent of the total number of parking stalls. Such parking stalls shall be marked "Compact Parking only" either on the parking surface or on a sign in front of the parking stalls.

*No Compact parking required therefore  
this section is met.*

3. Areas used for standing or maneuvering of vehicles shall have paved asphalt, concrete, solid concrete paver surfaces, or paved "tire track" strips maintained adequately for all weather use and so drained as to avoid the flow of water across sidewalks or into public streets, with the following exception:

a. The Planning Commission may approve the use of an engineered aggregate system for outdoor storage and/or non-required parking areas as part of a Conditional Use Permit provided that the applicant can demonstrate that City Standards related to:

- I. minimizing dust generation,
- II. minimizing transportation of aggregate to city streets, and
- III. minimizing infiltration of environmental contaminants including, but not limited to, motor oils, fuels, volatile organic compounds (e.g. benzene, toluene, ethylbenzene, xylene), and ethylene glycol are met.

The Planning Commission may impose conditions as necessary to meet City Standards.



b. Use of permeable surfacing materials for parking lots and driveways is encouraged whenever site and soil conditions make permeable surfacing feasible. Permeable surfacing includes, but is not limited to: paving blocks, turf block, pervious concrete, and porous asphalt. All permeable surfacing shall be designed, constructed, and maintained in accordance with the Canby Public Works Design Standards and the manufacturer's recommendations.

*The project will be using a combination of existing asphalt with the addition of new asphalt and permeable surfacing proposed for the enclosed parking area. Parking areas will drain into a catch basin and vegetated infiltration basin that will provide stormwater treatment prior to infiltration. The requirements of this section are met.*

4. The full width of driveways must be paved in accordance with (3) above:

a. For a minimum of 20 feet from the right-of-way line back into the private property to prevent debris from entering public streets, and

b. To within 150 feet of all portions of the exterior wall of the first story of any structure(s) served by the driveway to ensure fire and emergency service provision.

*The driveway will be fully and completely paved, therefore meeting this requirement.*

6. Groups of more than four (4) parking spaces shall be so located and served by driveways that their use will require no backing movements or other maneuvering within a street right-of-way other than an alley.

*The project does not require backing into any street right of way. Therefore, this requirement is met.*

7. Off-street parking areas, and the accesses to them, shall be designed and constructed to facilitate the flow of traffic, provide maximum safety of traffic access and egress and the maximum safety of pedestrian and vehicular traffic on the site and in adjacent roadways. The Planning Director or Planning Commission may require engineering analysis and/or truck turning diagrams to ensure safe and efficient traffic flow based on the number and type of vehicles using the site, the classification of the public roadway, and the design of the parking lot and access drives.

*The proposed parking and maneuvering layout of the site meet this requirement.*

8. Parking bumpers or wheel stops shall be provided to prevent cars from encroaching on the street right-of-way, adjacent landscaped areas, or adjacent pedestrian walkways.

*Parking bumpers and curbs are proposed to prevent cars from encroaching into adjacent landscape areas and pedestrian walkways. The provisions of this section are met.*

9. Accessible parking shall be provided, constructed, striped, signed and maintained as required by ORS 447.233 and all Oregon Structural Specialty Code requirements.

*Two accessible parking stalls are proposed. All will be striped and signed as required.*

## **B. Access.**

1. The provision and maintenance of vehicular and pedestrian ingress and egress from private property to the public streets as stipulated in this ordinance are continuing requirements

for the use of any structure or parcel of real property in the City of Canby. No building permit or other permits shall be issued until scale plans are presented that show how the ingress and egress requirement is to be fulfilled. Should the owner or occupant of a lot or building change the use to which the lot or building is put, thereby increasing ingress and egress requirements, it shall be unlawful and a violation of this ordinance to begin or maintain such altered use until the required increase in ingress and egress is provided.

*One existing driveway access point into the site is located along the southern property line off S. Township Rd. Ingress and egress for the proposed building will occur via this driveway. No access modifications are required to the public streets to serve this proposed development. The criteria of this section are met.*

2. The City of Canby encourages joint/shared access. Owners of two (2) or more uses, structures, or parcels of land may agree to, or may be required by the City to, utilized jointly the same ingress and egress when the combined ingress and egress of both uses, structures, or parcels of land satisfies their combined requirements as designed in this ordinance, provided that satisfactory legal evidence is presented to the City Attorney in the form of deeds, easements, leases or contracts shall be placed on permanent files with the city recorder.

*The proposed building will be the 4th building onsite and will jointly share one access point.*

3. All ingress and egress shall connect directly with public streets.

*The proposed ingress and egress will be via a curb cut to S. Township Rd. a public street. The requirement of this section is met.*

5. Required sidewalks shall extend from the ground floor entrances or the ground floor landing of a stairs, ramps or elevators to the sidewalk or curb of the public street or streets that provide the required access and egress.

*A sidewalk is proposed along the west side of the building connecting the public sidewalk to the ground floor entrance and parking stalls. The proposed sidewalks satisfy the requirement of this section.*

6. To afford safe pedestrian access and egress for properties within the city, a sidewalk shall be constructed along all street frontages, prior to use or occupancy of the building or structure proposed for said property. The sidewalks required by this section shall be constructed to city standards except in the case of streets with inadequate right-of-way width or where the final street design and grade have not been established, in which case the sidewalks shall be constructed to a design, and in a manner approved by the Site and Design Review Board. Sidewalks approved by Board may include temporary sidewalks and sidewalks constructed on private property; provided, however, that such sidewalks shall provide continuity with sidewalks of adjoining commercial developments existing or proposed. When a sidewalk is to adjoin a future street improvement, the sidewalk construction shall include construction of the curb and gutter section to grade and alignment established by the Site & Design Review Board.

*Existing sidewalks are located along the site's street frontage on S. Township Rd. This requirement is met.*

7. The standards set forth in this ordinance are minimum standards for access and egress, and may be increased through the site and design review process in any particular instance where the standards provided herein are deemed insufficient to protect the public health, safety and general welfare.

16.10.070(B)(9): Minimum access requirements for commercial or institutional uses - Ingress and egress for commercial uses shall not be less than the following:			
Parking spaces required	Minimum number of accesses required	Minimum access width	Sidewalks & curbs (in addition to driveways)
5-99	1	20 feet	Curbs required; sidewalk on one side minimum

*The project will share access to the site with the adjacent property to the west. Combined width will be over 20'. The existing access driveway is curbed on the south side of the project. Sidewalks will be constructed adjacent to the proposed building and leading back to the existing sidewalk to the south to provide an uninterrupted sidewalk / pathway network leading back to S. Township Rd. The requirements of this section have been met.*

12. Maximum driveway widths and other requirements except for single-family dwellings [see subsection (d) below]:

- a. Unless otherwise herein provided, maximum driveway widths shall not exceed forty (40) feet.
- b. No driveways shall be constructed within five (5) feet of an adjacent property line, except when two (2) adjacent property owners elect to provide joint access to their respective properties as provided by subsection 2.
- c. There shall be a minimum distance of forty (40) feet between any two (2) adjacent driveways on a single property.

*No new driveways to public streets are proposed, the standards of this section do not apply.*

13. Distance Between Driveways and Intersections- Except for single-family dwellings [see subsection (f) below] the minimum distance between driveways and intersections shall be as provided below. Distances listed shall be measured from the stop bar at the intersection:

- a. At the intersection of any collector or arterial streets, driveways shall be located a minimum of fifty (50) feet from the intersection.
- b. At the intersection of two (2) local streets, driveways shall be located a minimum of thirty (30) feet from the intersection as provided, the driveway shall be constructed as far from the intersection as possible, while still maintaining the five (5) foot setback between the driveway and property line.

*the standards of this section do not apply.*

**TABLE 16.10.070****Minimum Dimensional Standard for Parking**

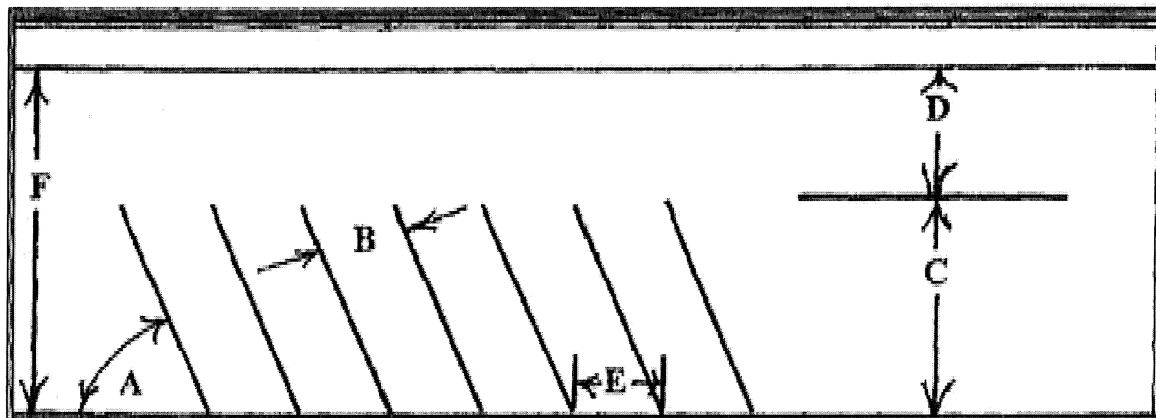
This table and Figure 16.10.070 provide the minimum dimensional standards for parking areas and spaces.

A = Parking angle in degrees D = Minimum clear aisle width

B = Minimum stall width E = Minimum clear stall distance at bay side

C = Minimum stall depth F = Minimum clear bay width

A	B	C	D	E	F
0 (parallel)	8'0"	-	12'0"	22'0"	20'0"
30	8'6"	16'4"	12'0"	17'0"	28'4"
45	8'6"	18'9"	12'6"	12'0"	31'3"
60	8'6"	19'10"	18'0"	9'10"	37'10"
90	8'6"	18'0"	24'0"	8'6"	42'0"



*Proposed parking stalls are both 90 and 45 degrees. All proposed parking stalls meet or exceed the minimum requirements of this section.*

**16.10.100 Bicycle Parking.**

Bicycle parking shall be provided for all multi-family residential, institutional, commercial, and industrial uses.

**A.** Dimensions and characteristics: Bicycle parking spaces shall be a minimum of six (6) feet long and two (2) feet wide, and overhead clearance in covered spaces shall be a minimum of seven (7) feet. A minimum five (5) foot aisle for bicycle maneuvering shall be provided and maintained beside or between each row of bicycle parking. Bicycle racks located on a sidewalk shall provide a minimum of two (2) feet between the rack and a wall or other obstacle, and between the rack and curb face. Bicycle racks or lockers shall be securely anchored to the surface or a structure. Bicycle racks located in the Downtown Commercial Zone shall be of the inverted U style (a.k.a. staple racks). See Figure 20 of the Canby Downtown Plan for correct rack placement.

**B. Location:** Bicycle parking shall be located in well-lit, secure locations within fifty (50) feet of the main entrance to a building, but not further from the entrance than the closest automobile parking space, and in no case further than 50 feet from an entrance when several entrances are involved.

**C. Number of spaces:** The bicycle parking standards set out in Table 16.10.100 shall be observed.

TABLE 16.10.100 BICYCLE PARKING STANDARD	
LAND USE CATEGORY	MINIMUM REQUIRED BICYCLE PARKING SPACES
Commercial	
Offices	2, or 1 space per 1000 ft <sup>2</sup> , whichever is greater
Industrial	
Warehouse	2 or .1 space per 1000 ft <sup>2</sup> , whichever is greater

*Per the uses listed above in Table 16.10.100, the office and warehouse portions of the site should be looked at separately. A minimum of 2 bicycle spaces will be required.*

*Office space: 1.0 space per 1,000 s.f. x 1,840 s.f. = 1.8 spaces required.*

*Warehouse space: 0.1 spaces per 1,000 s.f. x 2,791 s.f. = .27 spaces required*

*1.8 spaces + .27 spaces = 2.07 spaces required*

*2 bicycle parking spaces are proposed, meeting the minimum requirement.*

*The Site Plan indicates two locations for bike racks, both located inside the enclosed parking area north of building with each bike rack accommodating 2 bicycles. Each rack will be less than 50 feet from a building entrance and will be lit to the level of the adjacent parking lot. The requirements of this section have been met.*



## **16.32 M-1 LIGHT INDUSTRIAL ZONE**

### **16.32.010 Uses permitted outright.**

*Uses permitted outright in the M-1 zone shall be as follows:*

- A. *Manufacturing, fabricating, processing, compounding, assembling or packaging of products made from previously prepared materials such as cloth, plastic, paper, metal, wood (but not including sawmills or lumber mills), the operation of which will not result in*
  - 1. *The dissemination of dusts, gas, smoke, fumes, odors, atmospheric pollutants or noise which exceed Oregon Department of Environmental Quality standards*
  - 2. *Danger by reason of fire, explosion or other physical hazard;*
  - 3. *Unusual traffic hazards;*

*The M-1 zone allows for an electric sign fabrication and installation shop per (16.32.010 A)*

*Therefore the proposed use is permitted outright in the M-1 zone.*

### **16.32.030 Development standards.**

The following subsections indicate the required development standards of the M-1 zone:

- A. Minimum lot area: five thousand square feet;
- B. Minimum width and frontage: fifty feet;
- C. Minimum yard requirements:
  - 1. Street yard: twenty feet where abutting Highway 99-E and S. Ivy Street. Gas station canopies shall be exempted from the twenty foot setback requirements. Remaining property none, except ten feet where abutting a residential zone. Sign setbacks along Highway 99-E and S. Ivy Street are to be measured from the face of the curb rather than the lot line. Where no curb exists, the setback shall be measured from the property line. Other than signs which are nonconforming structures and street banners which have been approved per the requirements of the Uniform Sign Code, no signs will be allowed to be located within, or to project over, a street right-of-way.
  - 2. Interior yard: none, except ten feet where abutting a residential zone.

*The site does not abut Highway 99E, S. Ivy Street or a residential zone. A pole sign will be proposed. The minimum yard requirements of the M-1 zone are met*

- D. Maximum building height:
  - 1. Freestanding signs: thirty feet;
  - 2. All other structures: forty-five feet.

*The proposed building height is less than thirty five feet. A freestanding sign will be proposed. The maximum building height requirements of the M-1 zone are met.*

- E. Maximum lot coverage: no limit.

*The Code does not define lot coverage, but it is typically taken as the building area divided by the site area. The proposed building is 4,920 sq. ft., while the site area measures 21,831 sqft. for a lot coverage of 23 percent. The maximum lot coverage standards of the M-1 zone are met with the proposed building.*

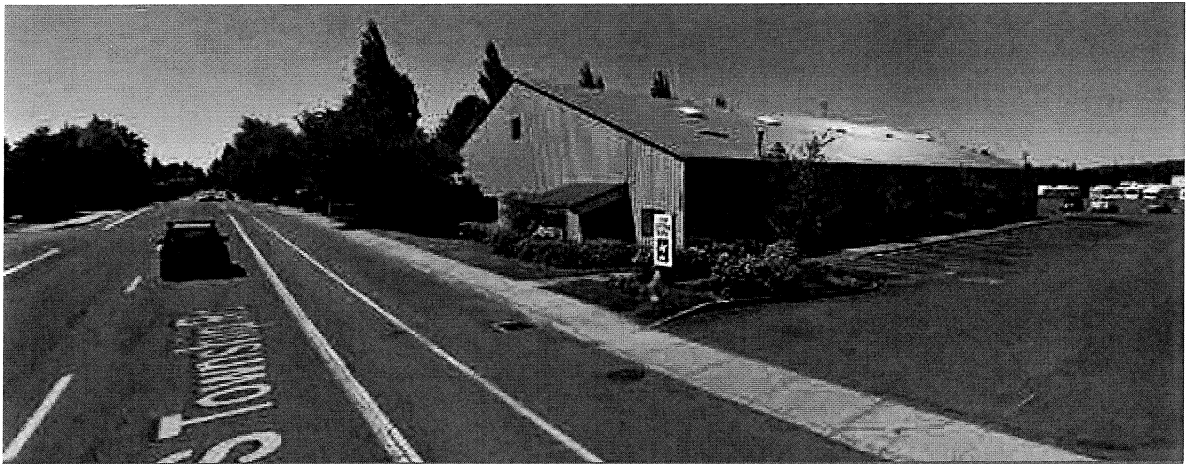
- F. Other regulations:

1. Vision clearance distances shall be fifteen feet from any alley or driveway and thirty feet from any other street or railroad.

*The vision clearance requirements at the existing driveway to S. Township Rd. is 30'. The existing driveway exceeds the minimum requirement, the proposed development will also have a 30' vision clearance triangle. The proposed building meets the requirement. A traffic study has been conducted by DKS Associates and paid for by the applicant.*

2. All setbacks to be measured from the foundation line of the building. Overhangs shall not exceed two feet.

*Building setbacks indicated on the Site Plan are from the property line to the foundation line. The proposed building also has a zero overhang and therefore meets these requirements.*



*Existing Hot Off The Press building located to the West. Building constructed with metal siding exterior walls and roofing. The southern elevation has a metal canopy that extends out to cover the building entrance, eastern facing elevation consists mostly of metal siding and a few windows.*

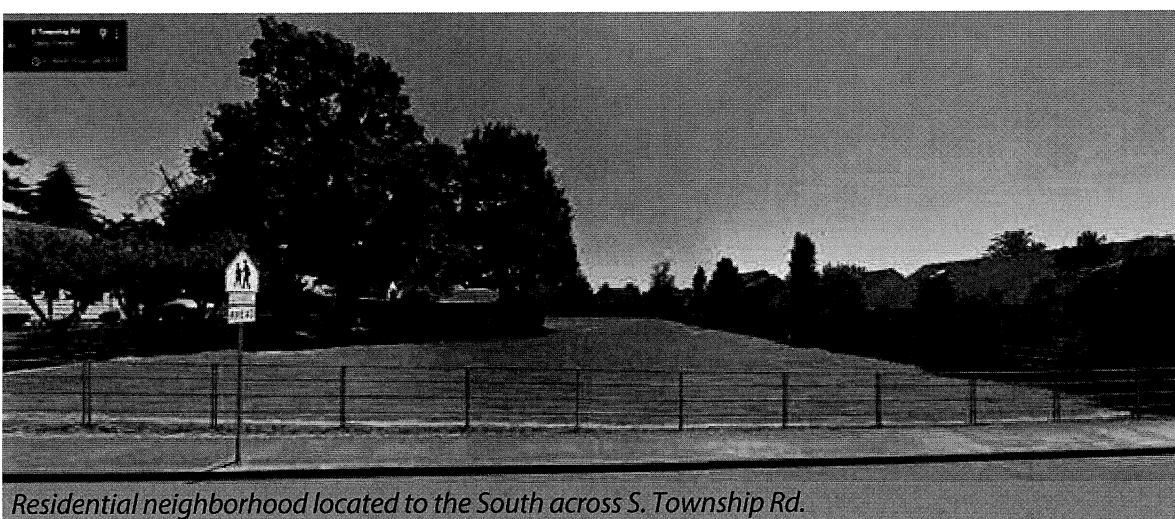


*Existing ACT Storage building located to the North. This building is constructed with horizontal composite siding and composite shingle roofing. The southern elevation has an entry door and two windows, the eastern and western elevations are mostly garage doors and horizontal siding.*





*Existing Clear Creek Forest Products building located to the East. Building constructed with vertical metal siding exterior walls and roofing. The southern elevation has a two exterior doors and two small windows. West and Eastern elevations are mostly siding with two windows.*



*Residential neighborhood located to the South across S. Township Rd.*

## 16.42 SIGNS

### 16.42.040 Design standards for signs.

The following standards apply to signs in all zone districts.

A. Setbacks. Signs are required to meet the setback requirements of the applicable zone district, except however the street yard setback for signs may be reduced to fifty (50) percent of that required for other structures in the zone. Signs shall not obstruct a vision clearance area required in the applicable zone district.

*No signs are proposed with this application. Any signage requested by future tenants will have to be approved by a separate process.*

## 16.43 OUTDOOR LIGHTING STANDARDS

### 16.43.040 Lighting Zones.

A. Zoning districts designated for residential uses (R-1, R-1.5 and R-2) are designated Lighting Zone One (LZ 1). All other zoning districts are designated Lighting Zone Two (LZ 2).

B. The designated Lighting Zone of a parcel or project shall determine the limitations for lighting as specified in this ordinance.

**Table 16.43.040 Lighting Zone descriptions**

Zone	Ambient Illumination	Representative Locations
LZ 1	Low	Rural areas, low-density urban neighborhoods and districts, residential historic districts. This zone is intended to be the default for residential areas.
LZ 2	Medium	High-density urban neighborhoods, shopping and commercial districts, industrial parks and districts. This zone is intended to be the default condition for commercial and industrial districts in urban areas.

*This commercial usage will be in the LZ (Lighting Zone) 2.*

### 16.43.060 Prohibited Light and Lighting.

A. All outdoor light sources, except street lights, shall be shielded or installed so that there is no direct line of sight between the light source or its reflection at a point 3 feet or higher above the ground at the property line of the source. Light that does not meet this requirement constitutes light trespass. Streetlights shall be fully shielded.

*Lighting will be installed to meet the requirements of this section. Cut sheets for proposed lighting are submitted with the application.*

B. The following lighting systems are prohibited from being installed or used except by special use permit:

1. Aerial Lasers.
2. "Searchlight" style lights.
3. Other very intense lighting, defined as having a light source exceeding 300 watts.

*None of the above lighting systems are proposed, the provisions of this section are met.*

#### **16.43.070 Luminaire Lamp Wattage, Shielding, and Installation Requirements.**

A. All outdoor lighting shall comply with the limits to lamp wattage and the shielding requirements in Table 16.43.070 per the applicable Lighting Zone. These limits are the upper limits. Good lighting design will usually result in lower limits.

B. The city may accept a photometric test report, demonstration or sample, or other satisfactory confirmation that the luminaire meets the requirements of the shielding classification.

C. Such shielded fixtures must be constructed and installed in such a manner that all light emitted by the fixture complies with the specification given. This includes all the light emitted by the fixture, either directly from the lamp or by a diffusing element, or indirectly by reflection or refraction from any part of the fixture. Any structural part of the fixture providing this shielding must be permanently affixed.

D. All canopy lighting must be fully shielded. However, indirect upward light is permitted under an opaque canopy provided that no lamp or vertical element of a lens or diffuser is visible from beyond the canopy and such that no direct upward light is emitted beyond the opaque canopy. Landscape features shall be used to block vehicle headlight trespass while vehicles are at an external point of service (i.e. drive-thru aisle).

E. All facade lighting must be restricted to the facade surface. The margins of the facade shall not be illuminated. Light trespass is prohibited. The sides of commercial buildings without a customer entrance shall not be lit.

**Table 16.43.070 - Luminaire Maximum Wattage and Required Shielding**

Lighting Zone	Fully Shielded	Shielded	Partly Shielded	Unshielded (Shielding is highly encouraged. Light trespass is prohibited.)
LZ 2	450	100	60	Landscape and facade lighting 100 watts or less; ornamental lights of 60 watts or less.

*Cut sheets for proposed lighting fixtures are included with the application. The applicant will install lighting to meet the requirements of this Code.*

#### 16.43.080 Height Limits.

Pole and surface-mounted luminaires under this section must conform with Section 16.43.070.

A. Lighting mounted onto poles or any structures intended primarily for mounting of lighting shall not exceed a mounting height of 40% of the horizontal distance of the light pole from the property line, nor a maximum height according to Table 16.43.080, whichever is lower. The following exceptions apply:

2. Lights specifically for driveways, and then only at the intersection of the road providing access to the site, may be mounted at any distance relative to the property line, but may not exceed the mounting height listed in Table 16.43.080.

3. Mounting heights greater than 40% of the horizontal distance to the property line but no greater than permitted by Table 16.43.080 may be used provided that the luminaire is side-shielded toward the property line.

B. Lighting mounted onto buildings or other structures shall not exceed a mounting height greater than 4 feet higher than the tallest part of the building or structure at the place where the lighting is installed, nor higher than 40% of the horizontal distance of the light from the property line, whichever is less. The following exceptions apply:

1. Lighting attached to single family residences shall not exceed the height of the eave. Lighting for driveways shall conform to Table 16.43.080.

2. Lighting for facades may be mounted at any height equal to or less than the total height of the structure being illuminated regardless of horizontal distance to property line.

3. For buildings less than 40 feet to the property line, including canopies or overhangs onto the sidewalk or public right of way, luminaires may be mounted to the vertical facade or the underside of canopies at 16 feet or less.

**Table 16.43.080 - Maximum Lighting Mounting Height in Feet**

Lighting Zone	Lighting for Driveways, Parking and Transit	Lighting for Walkways, Plazas and other Pedestrian Areas	All Other Lighting
LZ 2	37.5	18.0	16.0

*Exterior light fixtures will be mounted at the elevations shown on the Architectural Elevations. The applicant will install the outdoor lighting as required to meet the provisions of this section.*

#### 16.43.110 Lighting Plan Required

A lighting plan shall be submitted with the development or building permit application and shall include:

**A.** A site plan showing the location of all buildings and building heights, parking, and pedestrian areas.

**B.** The location and height (above grade) of all proposed and existing luminaires on the subject property.

**C.** Luminaire details including type and wattage of each lamp, shielding and cutoff information, and a copy of the manufacturer's specification sheet for each luminaire.

**D.** Control descriptions including type of control (time, motion sensor, etc.), the luminaire to be controlled by each control type, and the control schedule when applicable.

**E.** Any additional information necessary to demonstrate compliance with the standards in this section.

*A Site Lighting Plan, is submitted with the development application, meeting the requirements of this section.*

**16.49.040 Criteria and standards.**

**B.** In review of a Type III Site and Design Review Application, the Board shall, in exercising or performing its powers, duties or functions, determine whether there is compliance with the following:

1. The proposed site development, including the site plan, architecture, landscaping and graphic design, is in conformance with the standards of this and other applicable city ordinances insofar as the location, height and appearance of the proposed development are involved; and

2. The proposed design of the development is compatible with the design of other developments in the same general vicinity; and

3. The location, design, size, color and materials of the exterior of all structures and signs are compatible with the proposed development and appropriate to the design character of other structures in the same vicinity.

4. The proposed development incorporates the use of LID best management practices whenever feasible based on site and soil conditions. LID best management practices include, but are not limited to, minimizing impervious surfaces, designing on-site LID stormwater management facilities, and retaining native vegetation.

5. The Board shall, in making its determination of compliance with this Ordinance, shall use the matrix in Table 16.49.040 to determine compatibility unless this matrix is superseded by another matrix applicable to a specific zone or zones under this title. An application is considered to be compatible with the standards of Table 16.49.040 if the following conditions are met:

a. The development accumulates a minimum of 60 percent of the total possible number of points from the list of design criteria in Table 16.49.040; and

b. At least 10 percent of the points used to comply with (a) above must be from the list of LID Elements in Table 16.49.040. (Ord. 1338, 2010).

D. In review of a Type III Site and Design Review Application, the Board shall, in exercising or performing its powers, duties or functions, determine whether there is compliance with the INTENT of the design review standards set forth in this ordinance.

E. The Board shall, in making its determination of compliance with the above requirements, be guided by the objectives and standards set forth in this ordinance. It must be demonstrated that all required public facilities and services are available, or will become available through the development, to adequately meet the needs of the proposed development. If the site and design review plan includes utility facilities or public utility facility, then the City Planner shall determine whether those aspects of the proposed plan comply with applicable standards.

F. The Board shall, in making its determination of compliance with the requirements set forth, consider the effect of its action on the availability and cost of needed housing. The Board shall not use the requirements of this section to exclude needed housing types. However, consideration of these factors shall not prevent the Board from imposing conditions of approval necessary to meet the requirements of this section. The costs of such conditions shall not unduly increase the cost of housing beyond the minimum necessary to achieve the purposes of this ordinance.

G. As part of the site and design review, the property owner may apply for approval to cut trees in addition to those allowed in Chapter 12.32, the city Tree ordinance. The granting or denial of said application will be based on the criteria in Chapter 12.32. The cutting of trees does not in and of itself constitute change in the appearance of the property which would necessitate application for site and design review.

**Table 16.49.040 Site Design Review Menu**

As part of Site and Design Review, the following menu shall be used as part of the review. In Order to "pass" this table 60% of total possible points shall be earned,  
10% of the total possible points must be from LID elements

Design Criteria	Possible Points				
	0	1	2	3	4
<b>Parking</b>					
Screening of parking and/or loading facilities from public right-of-way	Not screened	Partially screened	Fully screened	-	-
Parking lot lighting provided	No	Yes	-	-	-
Parking location (behind building is best)	Front	Side	Behind	-	-
Number of parking spaces provided (% of minimum required)	>120%	101-120%	100%	-	-
<b>Total Points</b>					<b>6/7</b>
<b>Screening of Storage Areas and Utility Boxes</b>	<b>0</b>	<b>1</b>	<b>2</b>	<b>3</b>	<b>4</b>
Trash storage is screened from view by solid wood fence, masonry wall or landscaping.	No	Yes	-	-	-
Trash storage is located away from adjacent property lines.	0 - 10 feet from adjacent property	11 - 25 feet from adjacent property	>25 feet from adjacent property	-	-
Utility equipment, including rooftop equipment, is screened from view.	Not screened	Partially screened	Fully screened	-	-
<b>Total Points</b>					<b>4/5</b>
<b>Access</b>	<b>0</b>	<b>1</b>	<b>2</b>	<b>3</b>	<b>4</b>
Distance of access to nearest intersection.	≤70 feet	71 - 100 feet	>100 feet	-	-
Pedestrian walkways from public street/sidewalks to building entrances.	One entrance connected.	-	Walkways connecting all public streets/sidewalks to building entrances.	-	-
Pedestrian walkways from parking lot to building entrance.	No walkways	Walkway next to building only	Walkways connecting all parking areas to building entrances		
<b>Total Points</b>					<b>5/6</b>



Design Criteria	Possible Points				
Tree Retention	0	1	2	3	4
Percentage of trees retained	<10%	10-50%	51-75%	>75%	-
Replacement of trees removed	<50%	≥50%	-	-	-
<b>Total Points</b>					<b>1/4</b>
Signs	0	1	2	3	4
Dimensional size of sign (% of maximum permitted)	>75%	50-75%	<50%	-	-
Similarity of sign color to building color	Not similar	Somewhat similar	Similar	-	-
Pole sign used	Yes	No	-	-	-
<b>Total Points</b>					<b>3/5</b>
Building Appearance	0	1	2	3	4
Style (similar to surroundings)	Not similar	Somewhat similar (1 or 2 points possible depending on level of similarity)		-	-
Color (subdued and similar to surroundings is better)	Neither	Similar or subdued	Both	-	-
Material (concrete, wood and brick are best)	Either 1 or 2 points may assigned at the discretion of the Site and Design Review Board				
Size of building (smaller is better)	>20,000 square feet	<20,000 square feet	-	-	-
Provision of public art (i.e. murals, statues, fountains, decorative bike racks, etc.)	No	-	-	-	Yes
<b>Total Points</b>					<b>5/7</b>
Landscaping	0	1	2	3	4
Number of non-required trees provided	-	At least one tree per 500 square feet of landscaping.	-	-	-
Amount of grass (less grass is better) (% of total landscaped area)	>50%	25-50%	<25%	-	-
<b>Total Points</b>					<b>3/3</b>
Low Impact Development (LID)	0	1	2	3	4
Use of pervious paving materials (% of total paved area)	<10%	-	10-50%	51-75%	>75%
Provision of park or open space area	None	-	Open space (Generally not for public use)	-	Park (public or privately owned for public use)
<b>Total Points</b>					<b>4/8</b>



Design Criteria	Possible Points				
	0	1	2	3	4
Use of drought tolerant species in landscaping (% of total plants)	<25% drought tolerant	-	25-50% drought tolerant	51-75% drought tolerant	>75% drought tolerant
Provision of additional interior parking lot landscaping (% of minimum required)	100%	101-110%	111-120%	>120%	-
Provision of an eco-roof or rooftop garden (% of total roof area)	<10%	-	-	10-50%	>50%
Parking integrated within building footprint (below-grade, structured parking, or tuck-under parking) (% of total on-site parking)	<10%	-	-	10-50%	>50%
Disconnecting downspouts from city stormwater facilities	None	Some downspouts disconnected	All downspouts disconnected	-	-
Shared parking with adjacent uses or public parking structure (% of total required parking spaces)	None	<50%	≥50%	-	-
Provision of rain gardens/bioretention areas for stormwater runoff (% of total landscaped area)	None	-	10-50%	51-75%	>75%
Total Points	10/23				
As part of Site and Design Review, the following menu shall be used as part of the review. In Order to “pass” this table 60% of total possible points shall be earned, 10% of the total possible points must be from LID elements					

(Ord. 1296, 2008; Ord. 1338, 2010)

**Total Points Possible = 68**

**Total Points Achieved = 41**

**41 / 68 = 60%**

*The project is able to earn 60% of the total points achievable and 14 of 31 points (45%) came from the LID category. The requirements of the Site Design Review Matrix have been met.*

**16.49.065 Bicycle and pedestrian facilities.**

Developments coming under design review shall meet the following standards:

**A.** The internal walkway system shall be extended to the boundaries of the property to adjoining properties developed or zoned for commercial, public, or multi-family uses. The walkway shall connect to an existing walkway system on adjoining property or be located so as to provide for development of a logical connection in the future when the adjoining property is developed or redeveloped.

*The internal walkway system is being extended from the new building to the existing public sidewalk located along S. Township Rd.*

**B.** On-site facilities shall be provided to accommodate safe and convenient pedestrian and bicycle access within new subdivisions, multi-family developments, planned development, shopping centers, and commercial districts, and connecting to adjacent residential areas and neighborhood activity centers. Residential developments shall include streets with sidewalks and accessways.

*The provisions of this section do not apply to a single commercial parcel. To the extent practicable new walkways have been provided within the development.*

**C.** For new office parks and commercial development:

**1.** At least one sidewalk connection between the proposed development and each abutting commercial or office property shall be provided. One connection shall also be provided to each neighborhood.

**2.** Walkways shall be provided to the street for every 300 feet of developed frontage.

**3.** Walkways shall be direct with minimal driveway crossings.

**4.** Walkways shall be linked to the internal circulation of the building.

**5.** Walkways shall be at least five feet wide and shall be raised, or have different paving materials when crossing driveways or other vehicle maneuvering areas.

*All sidewalks are proposed to be at least 5 feet wide, linked to the internal circulation of the building and with minimal driveway crossings per the requirements of this section. One sidewalk connection is proposed between the proposed building and the sidewalk located existing public sidewalk located along S. Township Rd.*

*The provisions of this section are met.*

**D. Use of permeable surfacing materials for walkways is encouraged whenever site and soil conditions make it feasible. Permeable surfacing includes, but is not limited to, paving blocks, turf blocks, and porous asphalt. All permeable surfacing shall be designed, constructed, and maintained in accordance with the Canby Public Works Design Standards.**

**All walkways are proposed to be concrete.**

**16.49.080 General provisions for landscaping.**

**A. The standards set forth in this section are minimum standards for landscaping.**

**B. The purpose of these landscaping standards is to provide uniform standards for the development and maintenance of the landscaping of private property and public rights-of-way. The purpose of landscaping is to improve the livability of residential neighborhoods, enhance the customer attraction of commercial areas, increase property values, improve the compatibility of adjacent uses, provide visual separation and physical buffers between incompatible adjacent land uses, provide visual relief from the expanse of parking lots, screen undesirable views, contribute to the image and appeal of the overall community, and mitigate air and noise pollution. These standards are also intended to facilitate Low Impact Development (LID) techniques through the retention of existing native vegetation and mature, healthy trees, to the extent feasible. Additional LID related goals of this chapter are to: reduce erosion and storm water runoff; preserve and promote urban wildlife habitats; reduce the amount of carbon dioxide in the air; shade and reduce the temperature of adjacent waterways; and enhance the streetscapes along the city's public rights-of-way with an emphasis on trees and LID stormwater facilities.**

**C. The minimum area requirement for landscaping for developments coming under design review shall be the percentage of the total land area to be developed as follows. Parking lot landscaping area is included in calculating the following landscape areas:**

**1. Fifteen (15) percent for all industrial and commercial zones (except the Downtown-Commercial zone, but including the Commercial-Residential zone).**



2. Seven and one-half (7.5) percent for the Downtown-Commercial zone.
3. Thirty (30) percent for all residential zones.

*The minimum landscaping requirement of fifteen (15) percent of the site area has been met. The site area used in the calculation is newly developed and redeveloped portion of the site located near the proposed building.*

**D.** LID stormwater management facilities, such as rain gardens and bioretention areas, may be counted toward the minimum landscaping requirement when they are located on private property. LID facilities in the public right-of-way cannot be counted toward the minimum landscaping requirement. The integration of LID stormwater management facilities within required landscaping must be approved by the city and shall comply with the design and construction standards set forth in the Canby Public Works Design Standards.

*A stormwater infiltration reservoir will be located at the northwest corner of the property. There will also be a 5' wide landscaping berm on the south side of the property along S. Township Rd.*

**E.** Trees and other plant materials to be retained shall be identified on the landscape plan. The Site and Design Review Board encourages the retention, to the extent practicable, of existing healthy trees and vegetation.

*There are currently no trees on this site, only tall grass which will be removed for development.*

**F. During the construction process:**

1. The owner or the owner's agent shall provide above and below ground protection for existing trees and plant materials identified to remain.
2. Trees and plant materials identified for preservation shall be protected by chain link fencing placed around the tree, at the drip line.
3. If it is necessary to fence within the drip line, such fencing shall be specified by a qualified arborist, nurseryman or landscape architect.
4. Neither top soil storage nor construction material storage shall be located within the drip line of trees designated to be preserved.
5. Where site conditions make necessary grading, building, paving, trenching, boring, digging, or other similar encroachment upon a preserved tree's drip line area, such grading, paving, trenching, boring, digging or similar encroachment shall only be permitted under the direction of a qualified arborist, nurseryman or landscape architect. Such direction must assure that the health needs of trees within the preserved area can be met.
6. Tree root ends shall not remain exposed.

*No existing trees are proposed to be preserved. The provisions of this section do not apply.*



**G. Landscaping under preserved trees shall be compatible with the retention and health of said trees.**

*No existing trees are proposed to be preserved. The provisions of this section do not apply.*

**H. When it is necessary for a preserved tree to be moved in accordance with the Tree Ordinance, the landscaped area surrounding said tree or trees shall be maintained and replanted with trees which relate to the present landscape plan, or if there is no landscaping plan, then trees which are complimentary with existing, nearby landscape materials.**

*No existing trees are proposed to be preserved and moved. The provisions of this section do not apply.*

**I. Any required landscaped area shall be designed, constructed, installed and maintained so that within three (3) years, the ground shall be covered by living grass or other plant material. (The foliage crown of trees shall not be used to meet this requirement.) A maximum of five percent of the landscaped area may be covered with bark chips, mulch, or other similar materials. A maximum of five percent of the landscaped area may be covered with rock, stones, walkways, or other similar material acceptable to the Board. Required sidewalks shall not be used to meet the landscaping requirements.**

*A Landscaping Plan, meeting the requirements of this section, has been submitted with the application.*

**J. All trees and plant materials shall be healthy, disease-free, damage-free, well branched stock, characteristic of the species. The use of tree and plant species native to the Pacific Northwest is encouraged. Any new street tree planted must be included on the city's list of approved tree species.**

*A Landscaping Plan, meeting the requirements of this section, has been prepared by Aurora Landscape and submitted with the application.*

**K. Landscaping methods should be guided by the provisions of the most recent edition of the Sunset Western Garden Book or similar publication.**

*A Landscaping Plan, meeting the requirements of this section, has been prepared by Aurora Landscape and submitted with the application.*

**L. The following guidelines are suggested to insure the longevity and continued vigor of plant materials:**

**1. Select and site permanent landscape materials in such a manner as to produce a hardy and drought-resistant landscaped area.**

**2. Consider soil type and depth, spacing, exposure to sun and wind, slope and contours of the site, building walls and overhangs, and compatibility with existing native vegetation preserved on the site or in the vicinity.**



*A Landscaping Plan, meeting the requirements of this section, has been prepared by Scott Davidson Design LLC and submitted with the application.*

M. All plant growth in landscaped areas of developments shall be controlled by pruning, trimming or otherwise, so that:

1. It will not interfere with designated pedestrian or vehicular access; and
2. It will not constitute a traffic hazard because of reduced visibility.
3. It will not hinder solar access considerations.

*Site landscaping will be professionally maintained.*

N. After completion of site grading, topsoil is to be restored to exposed cut and fill areas to provide a suitable base for seeding and planting.

*Grading will be minimal, but a sufficient amount of topsoil will be placed on landscaping areas to provide for a suitable base for landscaping.*

O. All planting areas shall be graded to provide positive drainage.

*Planting areas will be graded away from the building to provide suitable drainage.*

P. Neither soil, water, plant materials nor mulching materials shall be allowed to wash across roadways or walkways.

*Landscape areas adjacent to walkways and driveways are generally curbed to prevent the material from washing. The exception to this is the landscaping area adjacent to the northern property line.*

#### **16.49.120 Parking lot landscaping standards.**

##### **C. Landscaping Within a Parking Lot.**

1. Area within a parking lot shall include the paved parking and maneuvering area, as well as any paved area within ten (10) feet of any exterior face of curb surrounding the paved parking and maneuvering area.

*The landscaped area within 10 feet of any exterior face of curb or paving/maneuvering area, was determined to be 1,761 sf.*

2. Each interior landscaped area shall be a minimum of six (6) feet wide, unless the area is added to the required perimeter landscaping.



3. The use of LID best management practices in parking lots is encouraged whenever site and soil conditions make it feasible. Such practices include, but are not limited to, permeable surfacing materials, and integrating LID stormwater management facilities into the required landscaping areas.

*LID stormwater management facilities are integrated into the landscaping areas of the site per the submitted Site Plan.*

**D. Computing Minimum Area Required to be Landscaped Within a Parking Lot.** Minimum area required to be landscaped within a parking lot shall be as follows:

1. Fifteen (15) percent for all residential, industrial, and commercial zones (except as provided below in subsections B and C).

*More than fifteen percent (15%) of the developed or redeveloped area of the site will be landscaped, nearly all in the area surrounding the parking lot. The provisions of this section have been met.*

**E.** All parking areas with more than 16 spaces shall include landscape islands to break up the parking area into rows of not more than 8 contiguous parking spaces.

1. Landscape islands shall have a minimum area of 48 square feet and a minimum width of six (6) feet.

2. Landscape islands shall contain at least one tree that meets the standards in subsection (6) below.

3. Landscape islands may be counted toward the minimum parking lot landscaping requirements.

*Does not apply*

**F. Criteria for Trees in Parking Lots.** Deciduous, evergreen and/or shade trees shall meet the following criteria:

1. Reach a mature height of forty (40) feet. Trees must be at least three-inch (3") caliper at the time of planting..

2. Cast moderate to dense shade in summer.

3. Be long lived, i.e., over sixty (60) years.

4. Do well in an urban environment:

a. Be pollution tolerant; and

b. Be tolerant of direct and reflected heat.

5. Require little maintenance:



- a. Be mechanically strong;
  - b. Be insect and disease resistant; and
  - c. Require little pruning.
- 6. Be resistant to drought conditions.
  - 7. Be barren of fruit production.

*The trees proposed on the Landscaping Plan meet the requirements of this section.*

**G. Perimeter of Parking and Loading Areas:**

1. Screening of parking and loading areas is required. Within three (3) years of planting, screening shall be of such height and density as to shield vehicle headlights from head-on visibility.

2. In addition, one (1) deciduous, evergreen and/or shade tree shall be planted every forty (40) feet, minimum, along the required setback of the vehicular use area.

*Screening of the parking and loading areas is proposed, including the use of shade trees located within the setbacks surrounding the vehicular use area. The requirements of this section have been met.*

**H. Irrigation System or Available Water Supply Required.** Landscaped areas shall be provided with automatic irrigation systems or a readily available water supply with at least one (1) outlet located within 150 feet of all plant materials to be maintained.

*Irrigation will be provided meeting the requirements of this section.*

## **Chapter 16.89 APPLICATION AND REVIEW PROCEDURES**

### **16.89.020 Description and Summary of Processes.**

All land use and development applications shall be decided by using the procedures contained in this Chapter. Specific procedures for each type of permit are contained in Sections 16.89.030 through 16.89.060. The procedure type assigned to each permit governs the decision-making process for that permit. Additional requirements may be found in the individual chapters governing each permit type. The four types of procedure are described below. Table 16.89.020 lists the City's land use and development applications and their required procedures.

**C. Type III Procedure (Quasi-Judicial/Legislative).** Type III decisions are made by the Planning Commission after a public hearing, with appeals reviewed by the City Council. Type III procedures generally use discretionary approval criteria.

*The applicant intends to have the application processed as a Type III Procedure.*



**TABLE 16.89.020**  
**Land Use and Development Application Procedures**

Application Type	Process Type	Notice Radius (Feet)	Neighborhood Meeting Required
Site and Design Review – Type III	III	600	Yes

*Site and Design Review – Type III Site and Design Review applications require a Neighborhood Meeting; mailers will be sent out and a neighborhood meeting will be scheduled to satisfy this requirement.*

**16.89.050 Type III Decision.**

- A. Pre-application conference.** A pre-application conference may be required by the Planning Director for Type III applications.

*Pre-application conferences for this project were held on 03/22/16. The requirements of this section have been met.*

- B. Neighborhood meetings.** As directed in Table 16.89.020, the applicant may be required to present their development proposal at a neighborhood meeting before the City accepts the application as complete. See Section 16.89.070.

*Mailers will be sent out and a neighborhood meeting will be scheduled to satisfy this requirement.*

- C. Application requirements.** Type III applications shall be made on forms provided by the Planning Director. The application shall be accompanied by all required information and fees.

*An application form has been provided for this project. The application form, the required fees and accompanying information were submitted to City of Canby Planning and Building. The requirements of this section have been met.*

*B. Type II Procedure (Administrative). Type II decisions are made by the Planning Director with public notice and an opportunity for a public hearing. The appeal of a Type II decision is heard by the Planning Commission.*



## **16.89.070 Neighborhood Meetings.**

**A.** Applicants are encouraged to meet with adjacent property owners and neighborhood representatives prior to submitting their application in order to solicit input, identify issues, and exchange information about the proposed meeting.

**B.** The Planning Commission or Planning Director may require an applicant to hold a meeting in the neighborhood prior to accepting an application as complete. A neighborhood meeting is required for some application types, as shown in Table 16.89.020, unless this requirement is waived by the Planning Director.

*Mailers will be sent out and a neighborhood meeting will be scheduled to satisfy this requirement.*

## **CONCLUSION**

*Through the preparation of this narrative and the other documents included with the Site and Design Review Application, the applicant has demonstrated compliance with the intent and requirements of the applicable Chapters of the City of Canby Land Development and Planning Ordinance. Therefore, the project should be approved.*

*Feel free to contact the following members of the Design Team if you have questions regarding the submittal:*

### **Applicant:**

Scott Davidson  
Scott Davidson Design, LLC  
7121 SE Carlton ST  
Portland, OR 97206  
(214)315-5302

### **Owner:**

Rudnick Holdings, LLC  
11401 S Finnegan's Way  
Oregon City, OR 97045  
(503)655-2610

### **Civil Engineering Consultant:**

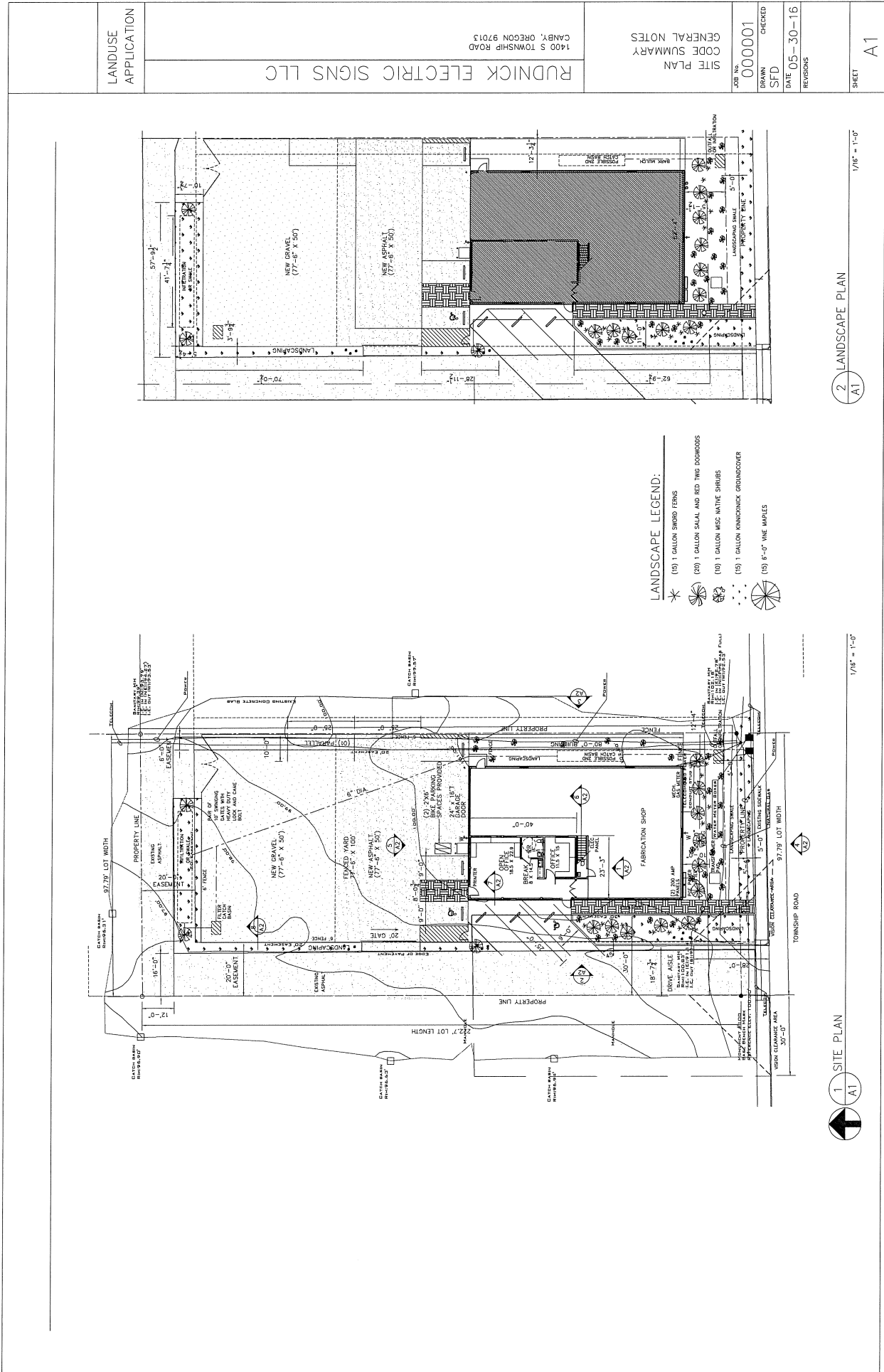
Paul Tibbot, P.E.  
Consulting Engineers  
1500 SW 11<sup>th</sup> Ave  
SUITE 1205  
Portland, OR 97021  
(503)310-9225

### **Architectural & Landscape Consultant:**

Scott Davidson Design, LLC  
7121 SE Carlton ST  
Portland, OR 97206  
(214)315-5302

### **Structural Engineer / Design Professional:**

Mark Ellingson, PE  
Pacific NorthWest Engineering, Inc.  
(503)873-3184



JOB No. 000001 DRAWN SFD DATE 05-30-16 REVISIONS	SITE PLAN CODE SUMMARY GENERAL NOTES	RUDNICK ELECTRIC SIGNS LLC 1400 S TOWNSHIP ROAD CANBY, OREGON 97013	LANDUSE APPLICATION



**\$179.50 free shipping**

**Please call for credit card payments**



**217-806-0502**

\*\*\*Due to government regulations this fixture can't be delivered to California\*\*\*

***We are Ebay's #1 seller of commercial and industrial lighting products. Having sold on Ebay since 1998, we know the importance of offering quality products at low prices.***

**Included in this auction:**

**New 400 Watt Metal Halide Parking Lot Light PULSE  
START**

**Yoke Mount/U-Bracket**

**Free Bulb**

**Free Shipping**

# ***These lights are ideal for mounting to a wood pole or to the side of a building.***

## **Features:**

HID Floodlight with quad-tap CWA ballast. Includes 400-watt PSMH lamp.

**Applications** - Building facades and displays. Also effective as security lighting, for storage areas and commercial sites

These lights are UL listed and conform with new government regulation requiring Pulse Start type ballasts and lamps. Pulse Starts lights put off more light and require less maintenance over time, as the lamps last longer and maintain their brightness for longer periods of time than tradition probe start lamps.

16-1/2-in H x 17-1/4-in W x 7-1/2-in D  
Typical mounting height: 15 to 30 feet

---



Lamp Output: 39000 lumens  
Lamp Color Rendition: 70 CRI

- **Yoke Mount/U-bracket for mounting to side of building or wood pole**
- Quad-tap CWA ballast (HPF)
- Sturdy die-cast aluminum housing and frame, with dark-bronze polyester powder-coat finish.
- Tempered-glass lens, thermal-shock and impact resistant
- Typical Spacing 2 to 4 times the mounting height.
- UL Listed for wet location
- 1-year warranty

**Please call for credit card payments**



**217-806-0502**

Frequently Asked Questions:

1) **What brand are these light fixtures?** These lights are manufactured by Howard Lighting, based in Laurel MS. *The quality is superb. We have sold these lights for over 6 years and have not had any customers disappointed with their quality. Check out our feedback :)*

2) **Do they come with a warranty?** Yes, *these lights are warrantied for 1 year. No hassle warranty! In addition, we warranty their arrival to you in working condition and not damaged.*

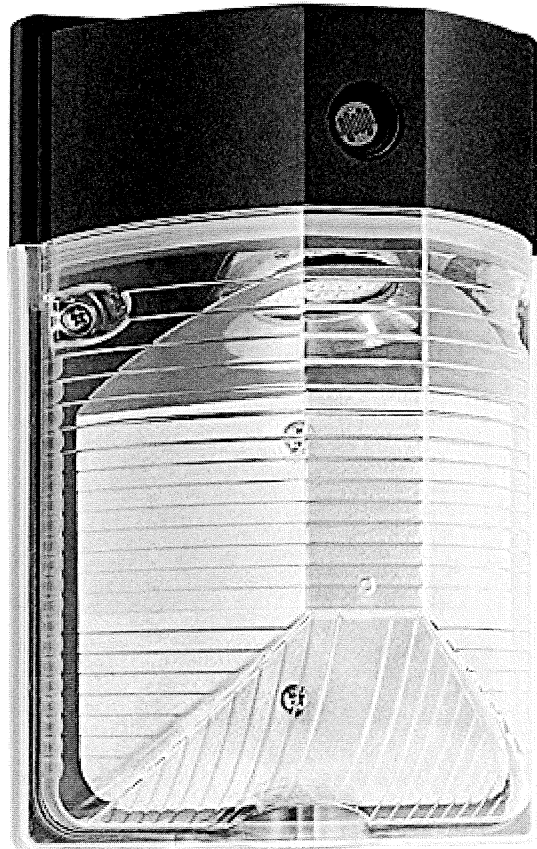
3) **What voltage are these lights wired for?** *These lights have multi-tap ballasts and can be used for 120/208/240/277 volt applications.*

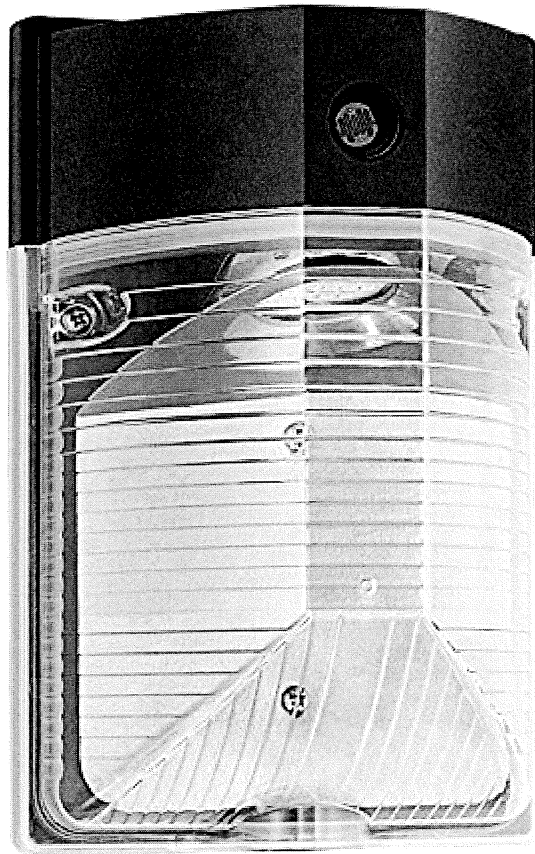
4) **What is the basic difference between metal halide and high pressure sodium lighting?** *Metal halide is best described as a "white" light; high pressure sodium is best described as a "yellow/gold" light.*

**5) When will my lights ship?** Orders are normally shipped within 1-2 business day of payment.

**6)How do I wire and mount these?** It is always recommended for these to be installed by a licensed and experienced electrician. A u-bracket/yoke is included which allows this light to be fasted to any solid surface such as the side of a building or wood pole. Lights can be wired for 120, 208, 240, or 277 volts. Lights are prewired for 277 volts, so please be sure to open the light to wire to the corresponding ballast voltage you need.

**25W LED Wall Mount Light, DLC & UL-listed Dusk to Dawn LED Wall Pack, 150-200W MH and HPS replacement, 2250lm, 120V, 5000K Daylight, Exterior/Outdoor/ Entrance Security Light (5-Year Warranty)**

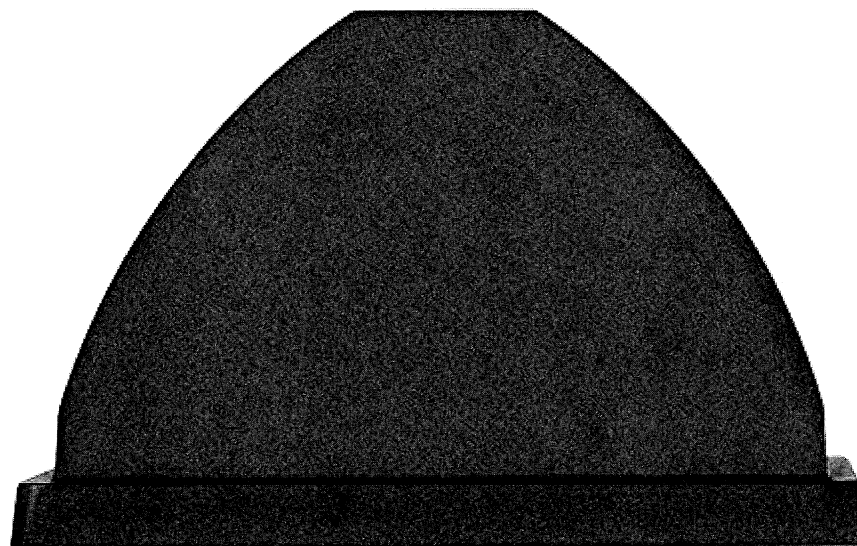








Die-casting housing for excellent heat dissipation



[CLICK TO ENLARGE](#)

25W LED Wall Mount Light, DLC & UL-listed Dusk to Dawn LED Wall Pack, 150-200W MH and HPS replacement, 2250lm, 120V, 5000K Daylight, Exterior/Outdoor/ Entrance Security Light (5-Year Warranty)

## DESCRIPTION

This is a brand new: 25W LED Wall Mount Light, DLC & UL-listed Dusk to Dawn LED Wall Pack, 150-200W MH and HPS replacement, 2250lm, 120V, 5000K Daylight, Exterior/Outdoor/ Entrance Security Light (5-Year Warranty)

The brand-new photocell LED wall mount light from LeonLite is the ideal equipment when it comes to outdoor lighting. The 5000K daylight LED lamp produce a brighter and more realistic light source and use less than 25% of the traditional 150-200W MH and HPS and can be used for over 50,000hrs under care.



Dusk to dawn photocell technology automatically turns on/off by sensing its surrounding's light. No more fishing for your keys in the dark or step your feet on the ground hard to activate the light on your front porch any more, the darker the surrounding, the brighter it shines, for extra convenient, safety and protection on you and those you love.

It's perfectly sealed lens prevent dust and those annoying little bugs from getting inside the lamp and causing flickers on the bulb, no more dust and dead flies piling up inside your lamp anymore! The die-casting shatter and corrosion proof housing allows the lamp to be safely use in wet location, perfect for security, entry ways, stairways, storage and perimeter areas, as well as residential exteriors.

5 years warranty both DLC and UL listed for its premium quality and performance, only products that meet the rigorous qualities and standards can be listed with these titles. Products that were qualified by the DLC are also eligible for participating in rebate programs.

**Note:** Input voltage 120V, please mount under proper input voltage for this product.

#### **Specifications**

Wattage: 25W

Voltage: 120V

Lumens: 2250lm

CCT: 5000K daylight

CRI: >80

Finished color: Dark bronze




Lifespan: 50,000 hours

Working temperature: -13°F-104 °F

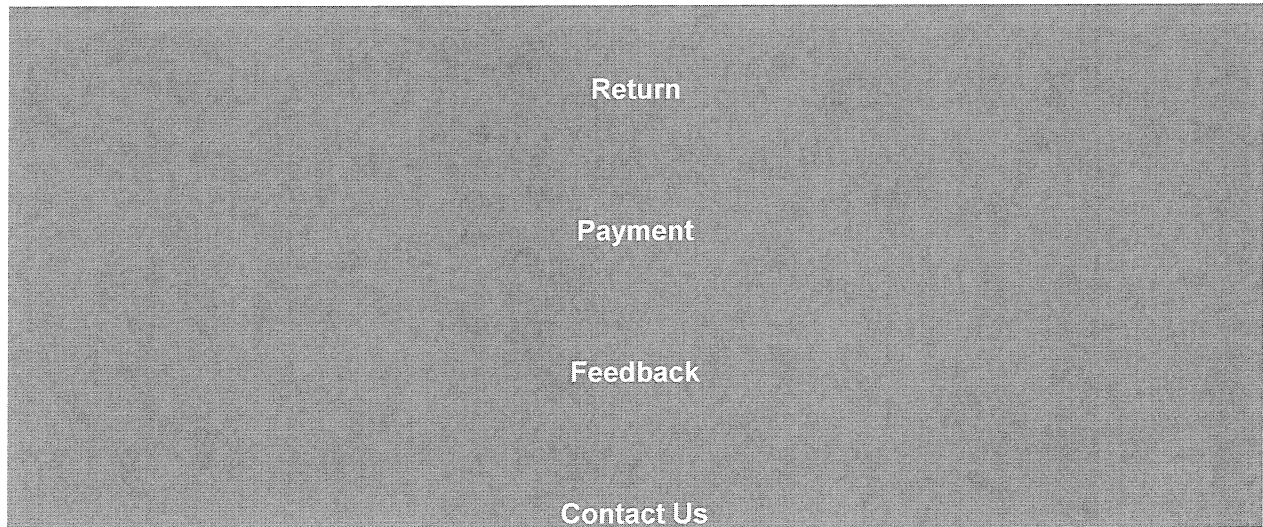
Dimension: 8.38 x 5.48 x 3.52 inch

- Consume only 25W, perfect replacement for the standard 150 to 200W metal halide and high pressure sodium fixtures, light up constantly for over 50,000 hours.
- Dusk to dawn photocell technology, automatically turns on/off when the surrounding gets dark/brighten, perfect for security, entry ways, stairways, storage and perimeter areas, as well as residential exteriors.
- DLC-qualified for rebate programs and UL listed top-notch quality for your safe operation and best performance, with a 5 years warranty.
- Die-casting housing for an excellent cooling system & anti-corrosion; UV-stabilized polycarbonate clear lens safe for using in outdoor and wet application.
- 2250lm, 5000K LED light gives you a more bright and vivid light source, input voltage 120V.

This spec. is only relevant for this item: 25W LED Wall Mount Light, DLC & UL-listed Dusk to Dawn LED Wall Pack, 150-200W MH and HPS replacement, 2250lm, 120V, 5000K Daylight, Exterior/Outdoor/ Entrance Security Light (5-Year Warranty)

 <b>FAST SHIPPING</b>		 <b>JOIN MAIL</b>
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**Shipping**



**Domestic Shipping: All Domestic packages are shipped Ground Shipping For Free (Unless other shipping method was selected on the order)**

- We are now shipping to worldwide addresses, the item is sent to a logistic center in KY and shipped from there to the rest of the world
- International customers are responsible for all duties and taxes
- Contact your local authority for questions related to the amount of duties or taxes you will be responsible for
- We ship within two business days of payment, usually sooner
- Standard FREE shipping takes between 3-5 business days
- We use a selection of shipping services such as UPS, FedEx, USPS etc.
- Expedited shipping takes between 2-3 business days
- Overnight shipping takes between 1-2 business days (Ships within 12 hours of payment)
- We do not accept P.O. BOXES -PLEASE PROVIDE A PHYSICAL ADDRESS
- Shipping this item: 25W LED Wall Mount Light, DLC & UL-listed Dusk to Dawn LED Wall Pack, 150-200W MH and HPS replacement, 2250lm, 120V, 5000K Daylight, Exterior/Outdoor/ Entrance Security Light (5-Year Warranty)



720 SW Washington St.  
Suite 500  
Portland, OR 97205  
503.243.3500  
www.dksassociates.com

## MEMORANDUM

**DATE:** June 15, 2016

**TO:** Bryan Brown, City of Canby

**FROM:** Chris Maciejewski, PE, PTOE  
Nate Schroeder, PE, PTOE



**SUBJECT:** Canby Rudnick Electric Signs Traffic Impact Study (TIS)

P#11010-066

This memorandum summarizes the potential transportation impacts associated with the proposed development of a manufacturing facility in Canby, Oregon. The project site is located at 1400 SW Township Road, on Lot 2 of the Rustbuster Subdivision. The 0.5 acre lot is currently undeveloped, and zoned Light Industrial (M-1)<sup>1</sup>. The proposed development site will house a sign contracting design and manufacturing company, with a planned manufacturing building space of 4,000 square feet.<sup>2</sup>

Access to the site will be provided by SE Township Road, which is classified as a collector roadway by the City of Canby. The proposed development would not generate enough traffic to warrant an off-site transportation impact analysis. Therefore the following sections describe the site conditions only, including estimated project trip generation and a review of the site access (spacing and sight distance).

### Project Trip Generation

The amount of new vehicle trips generated by the proposed land use was estimated using trip generation estimates published in the ITE Trip Generation Manual for a similar land use.<sup>3</sup> Trip generation estimates for the existing land use and proposed development are provided for daily, morning, and evening peak hours, and are summarized in Table 1. Since the additional morning and evening peak hour trip generation is relatively low, no off-site impacts were evaluated.

---

<sup>1</sup> City of Canby, Current Zoning Map, June, 2009.

<sup>2</sup> Based on site plan provided by Rudnick Electric Signs LLC

<sup>3</sup> Trip Generation Manual, Institute of Transportation Engineers, 9<sup>th</sup> Edition.

**Table 1: Vehicle Trip Generation Estimate**

Land Use (ITE Code)	Units	Total Daily Trips	Time	Trip	Peak Hour Trips		
			Period	Generation Rate	In	Out	Total
General Light Industrial (110)	4 Ksf	28	AM Peak Hour	0.92 trips/unit	4	0	4
			PM Peak Hour	0.97 trips/unit	0	4	4

The following sections discuss the evaluation of the site access and sight distance.

## Site Access

Access to the site is currently provided by a common shared driveway along S Township Road, which is classified as a collector roadway. The existing access will continue to be used for site access in the future. S Township Road currently meets the City's cross-section requirements for standard collector streets (34-50 feet paved with 50-80 feet of ROW) and should safely accommodate additional vehicle, pedestrian, and bicycle traffic.

The pedestrian and bicycle improvement plans provided in the city's TSP<sup>4</sup> do not identify any pedestrian or bicycle projects in the site vicinity. The proposed on-site pedestrian facilities and their connection to facilities along S Township Road appear to be adequate.

## Sight Distance

Based on a posted speed of 25 mph, the American Association of State Highway and Transportation Officials (AASHTO) sight distance requirements specify that 240 feet and 280 feet for right and left turning vehicles, respectively, should be provided.<sup>5</sup> Existing intersection sight distance measurements are summarized in Table 2.

<sup>4</sup> Canby Transportation System Plan (TSP), December 2010; Figure 5-1 (Pedestrian Improvements) and Figure 6-1 (Bicycle Improvements).

<sup>5</sup> AASHTO – *Geometric Design of Highways and Streets*, 2004. Exhibit 9-55. Design Intersection Sight Distance – Case B1 – Left Turn from Stop, and Exhibit 9-58. Design Intersection Sight Distance – Case B2 – Right Turn from Stop and Case B3 – Crossing Maneuver.



**Table 2: Intersection Sight Distance Evaluation**

Access Location	Posted Speed	Turning Direction	Required Intersection Sight Distance	Available Intersection Sight Distance	Sight Distance Adequate?
Existing Driveway	25 mph	Right	240 feet	>350 feet	Yes
	25 mph	Left	280 feet	>350 feet	Yes

## FINDINGS

Based upon the analysis presented in this memorandum, the proposed project was found to not generate off-site traffic impacts under a reasonable worst-case trip generation scenario. The proposed development would generate an additional four net new trips in the morning peak period and four net new trips in the evening peak period. Therefore, no off-site mitigation is recommended for the proposed project as a result of traffic impacts.

If you have any questions, please feel free to call or email.

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119/21

# RUSTBUSTERS

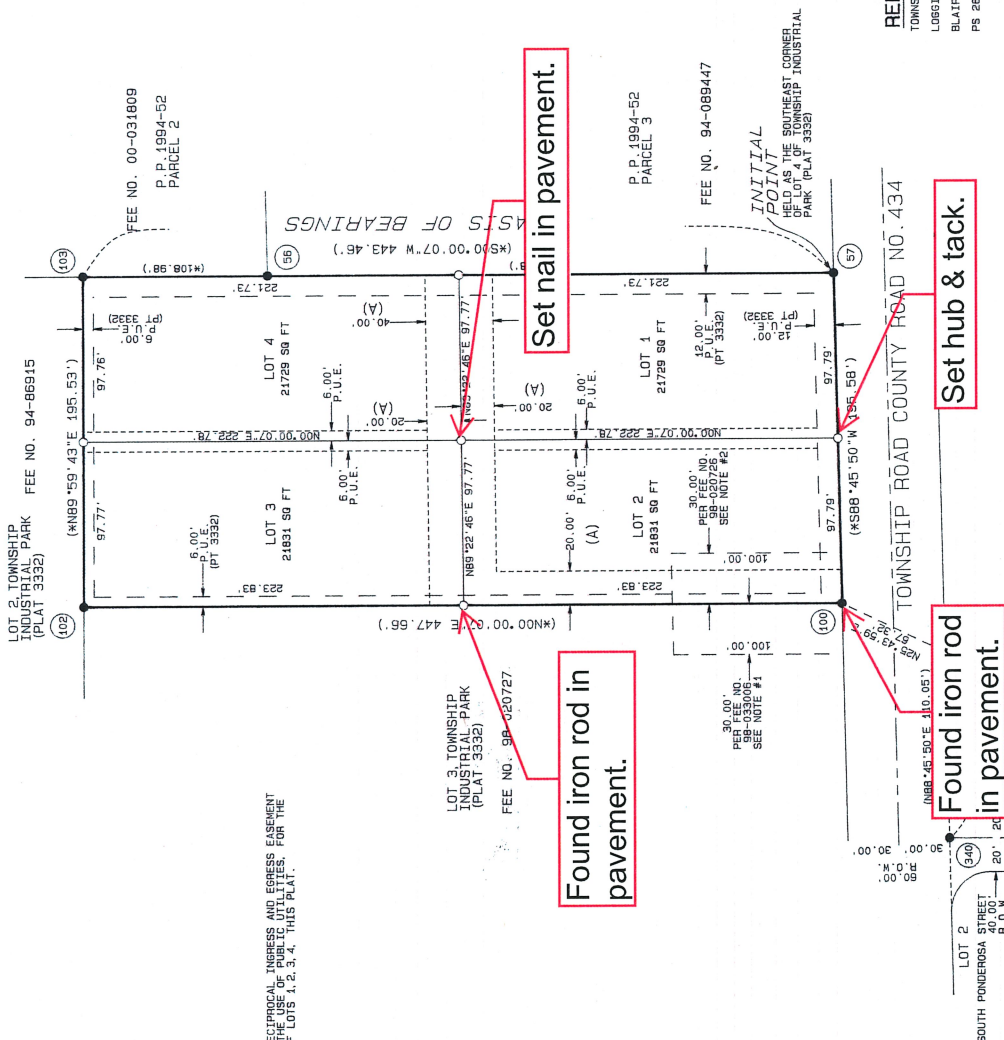
A REPLAT OF LOT 4, TOWNSHIP INDUSTRIAL PARK (PLAT 3332); AND THE PHILANDER LEE D.L.C. 56; AND A PORTION OF GOVERNMENT LOT 5, IN THE S.W. 1/4 OF SECTION 34, TOWNSHIP 3 SOUTH, RANGE 1 EAST; WILLAMETTE MERIDIAN, CITY OF CANBY, CLACKAMAS COUNTY, OREGON.  
CITY OF CANBY PLANNING FILE NO. SUB 00-03/DR 00-04  
DATE OF SURVEY: SEPTEMBER 12, 2000



NORTH  
1"=50'

## NOTE:

(A) - DENOTES RECIPROCAL INGRESSES AND EGRESS EASEMENT ALSO FOR THE USE OF PUBLIC UTILITIES, FOR THE BENEFIT OF LOTS 1, 2, 3, 4. THIS PLAT.



## MONUMENT IDENTIFICATION

(ALL MONUMENTS FOUND ARE FLUSH WITH THE GROUND AND IN GOOD CONDITION UNLESS OTHERWISE NOTED.) + OR - DISTANCE AFTER MONUMENT DESCRIPTION IS TOP OF MONUMENT, + ABOVE SURFACE, - BELOW SURFACE.

- (56) FOUND, 5/8" IRON ROD, FLUSH WITH SURFACE, WITH YELLOW PLASTIC CAP MARKED "WILHELM ENG OR LS 2413" PER P.P. 1994-52.
- (57) FOUND, 5/8" IRON ROD, FLUSH WITH SURFACE, WITH YELLOW PLASTIC CAP MARKED "WILHELM ENG OR LS 2413" PER P.P. 1994-52.
- (100) FOUND, 5/8" IRON ROD, FLUSH WITH SURFACE, WITH YELLOW PLASTIC CAP MARKED "WILHELM ENG OR LS 2413" PER PS 28994, HELD PER OUTBOUNDS SURVEY FOR RUSTBUSTERS SURFACE.
- (102) FOUND, 5/8" IRON ROD, FLUSH WITH SURFACE, WITH YELLOW PLASTIC CAP MARKED "WILHELM ENG OR LS 2413" PER PLAT 3332, HELD.
- (103) FOUND, 5/8" IRON ROD, FLUSH WITH SURFACE, WITH YELLOW PLASTIC CAP MARKED "WILHELM ENG OR LS 2413" PER PLAT 3332, HELD.
- (340) FOUND, 5/8" IRON ROD, FLUSH WITH SURFACE, WITH YELLOW PLASTIC CAP MARKED "WILHELM ENG OR LS 2413" PER PLAT 3457, HELD.
- (341) FOUND, 5/8" IRON ROD, FLUSH WITH SURFACE, WITH YELLOW PLASTIC CAP MARKED "WILHELM ENG OR LS 2413" PER PLAT 3457, HELD.

## LEGEND

- DENOTES MONUMENT FOUND AS IDENTIFIED
- DENOTES 5/8" X 30" IRON ROD SET WITH A YELLOW PLASTIC CAP MARKED "WILHELM ENG. OR. LS 2413" MONUMENTS SET ON NOVEMBER 26, 2000
- (\*) DENOTES MEASURED AND RECORDED DATA FROM PS 28994, OUTBOUNDS SURVEY, OR AS NOTED
- PT DENOTES PLAT
- P.P. DENOTES PARTITION PLAT
- R.O.W. DENOTES RIGHT OF WAY
- P.U.E. DENOTES PUBLIC UTILITY EASEMENT
- DENOTES ADJACENT CENTERLINE
- DENOTES PUBLIC UTILITY EASEMENT LINES AS NOTED
- DENOTES PUBLIC UTILITY EASEMENT LINES (THIS PLAT)

I HEREBY CERTIFY THIS MAP IS AN EXACT COPY OF THE ORIGINAL PLAT

REGISTERED  
PROFESSIONAL  
LAND SURVEYOR

OREGON  
JULIUS R. WILHELM  
2413  
RENEWED 7/1/02

## REFERENCE SURVEYS

- TOWNSHIP INDUSTRIAL PARK (PLAT 3332)
- LOGGING ROAD INDUSTRIAL PARK (PLAT 3054)
- BLAIR COMMONS (PLAT 3457)
- PS 28994 OUTBOUNDS SURVEY (RUSTBUSTERS)

WILHELM ENGINEERING, INC.  
ENGINEERS & SURVEYORS  
PLANNING  
DESIGN  
CONSTRUCTION  
PHOTOGRAPHY  
LAYER LIST: SUB-PLAT  
PROJ: 200029

3649



## **CITY OF CANBY –COMMENT FORM**

If you are unable to attend the Public Hearing, you may submit written comments on this form or in a letter addressing the Planning Commission. Please send comments to the City of Canby Planning Department:

**By mail:** Planning Department, PO Box 930, Canby, OR 97013  
**In person:** Planning Department at 111 NW Second Street  
**E-mail:** [brownb@ci.canby.or.us](mailto:brownb@ci.canby.or.us)

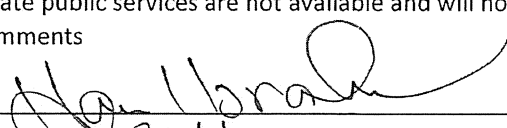
Written comments to be included in the Planning Commission's meeting packet are due by Noon on Thursday, July 14, 2016. Written comments can also be submitted up to the time of the Public Hearing on Monday, July 25, 2016 in person to the Planning Commission during the Public Hearing at 7 pm in the City Council Chambers, 155 NW 2<sup>nd</sup> Avenue.

***Application: Site & Design Review – DR 16-04 Rudnick Electric Signs Commercial Office/Fabrication Shop***  
**COMMENTS:**

1. WE SUGGEST THE BROKEN PORTION OF THE CONCRETE DRIVEWAY  
BE REMOVED AND REPLACED.

***Please check one box and fill in your Name/Agency/Date below:***

- ☐ Adequate Public Services (of your agency) are available  
☒ Adequate Public Services will become available through the development  
☒ Conditions are needed, as indicated  
☐ Adequate public services are not available and will not become available  
☐ No Comments

NAME:   
AGENCY: CM  
DATE: 7/11/2016

**Thank you!**



Current location: 1625 Washington St. Oregon City, OR. 97045

Ph: 503-655-2610 – Fax: 503-980-7919 – e-mail: [steve@rudnickelectricsigns.com](mailto:steve@rudnickelectricsigns.com)

Rudnick Electric Signs LLC:

Hello, I am Steve Rudnick, owner & master sign electrician licensed in the states of OR. & WA. We are opening a UL listed sign shop.

It is required that we hold a meeting even for the Industrial zoned property located at **1400 SE Township road, Canby, OR. 97013**. **It is not required that you attend this meeting** but we will be recording minutes, answering and recording any questions or comments about our property & building that is going here. You can view the attached construction documents that pertain to the site.

The **meeting is set** to be at the **1400 township road site** outside on the grass. **If** it is **raining**, then we will **relocate** to our current **office in Oregon City** at the above address on **6-08-16 Friday from 4pm to 5pm**.

#### About our business:

We consider our self to be a great asset to the community, already contributing taxes while maintaining a business license in the city of Canby. We have been doing work in Canby & the surrounding Portland metro area for many years. We have established a relationship with the city and local businesses such as Canby Builders supply, Orielly Auto Parts and many others to name a few. We have bucket trucks, cranes and offer lighting maintenance and construction services as well as manufacturing all sign types.

#### About the dealings with the city:

We are trying to make the best use possible of our area for an industrial use with retail feel and meet codes as close as possible with out to many hiccups. I need your help so we can have the best use of our property while coming close to meeting codes but exceeding what most existing surrounding industrial business have. We are doing allot to make this a nice area but the easements and the landscaping area required by the city has forced us to give up making a building that is large enough to lease tenant spaces so we are likely to be the only tenant and is our current plan.

#### About our building:

We will have exterior & interior parking for ease for our customers & employees. A side walk will be extended from the cities to our front door. We will have a slated fence around the perimeter of our yard area which will be on the back side of our building away from the street. Most employees will park inside of the gated area. Our landscaping is just over the 15% needed and will be native so we only have to water it for 1 year. Having a steel roof instead of composite is environmentally friendly because the water run off on a steel roof is drinkable while the run off for a composite roof is not. This will be a stick framed building with wood siding, coffee colors.

# Rudnick Electric Sings LLC Community Meeting Notes

## July 7, 2016 4:00 PM

Attendees list attached.

Steve set up tape showing where the building would approximately be on the lot.

Steve started off by explaining that he has a sign company that spends most of their time on the site of various customers throughout the area. He also discussed where the parking lot, fence and landscaping will be placed.

John Pigott made the point that Township road is very busy during the school year with children walking to and from school as well as school buses and parents picking up children. *Steve explained that he is open from 8 until 5. He has 6 employees and runs 5 trucks that usually leave in morning and return at the end of the day. He has very few customers that come to his place of business.*

John also asked about the noise level. *Steve explained that they run a pop riveter and bend metal. They do the work during business hours and only on very rare occasions work in the evening or weekends. The building will be insulated which should help mitigate any noise caused by machinery.*

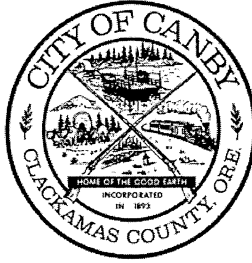
Peter Hostetler asked about whether there would be a monument sign and where the gates for the yard would be. *Steve said that he does plan on a monument sign in the future and will keep Peter informed of the plans as they develop. He showed Peter on the drawings where the gates are planned to be placed.* Peter mentioned that one of the other businesses does have delivery trucks that may interfere at times with the north gate. *Steve said that he would work with him as needed.*

Mary and Robert from 1429 SE Township arrived late and were glad to see that the property was going to be developed. They were wondering why the move, how many employees, and whether there was going to be an open house. *Steve explained that he is currently renting space for his business and that the math works better for him to own his own building. He repeated what he said above regarding employees as well as explained that he has designers, installers as well as other that help make his business run. He also explained that employees often have multiple areas of expertise. Steve said he will keep the neighbors informed as to any open house and also that his door is always open.*

*Steve talked about his clients are both local business as well as national chains that are in the area.*

## Rudnick Electric Signs LLC Community Meeting July 7, 2016

[illegible]



## **Pre-Application Meeting**

**1400 SE Township Road**

**March 22, 2016**

**10:30 am**

### **Attended by:**

Scott Davidson, Steve Runick, 214-315-5302  
Hassan Ibrahim, Curran-McLeod Engineering, 503-684-3478  
Doug Quan, Canby Utility, Water Dept, 971-563-6314  
Gary Stockwell, Canby Utility, Electric Dept, 503-263-4307  
Larry Hepler, Canby Utility, 971-563-1375  
Nathan Parrish, NJP Construction, 503-780-8534

David Riener, Steve Runick, 971-263-3735  
Dan Mickelsen, Erosion Control, 503-266-0698  
Jerry Nelzen, Public Works, 971-253-9173  
Mark Gunter, Public Works, 541-231-8674  
Bryan Brown, Planning Dept, 503-266-0702  
Steven Rudnick, Owner, 503-997-8882

***This document is for preliminary use only and is not a contractual document.***

### **SCOTT DAVIDISON**

- This is a new development for Steve Rudnick and we will be constructing a single story pole barn type structure with a concrete foundation and mezzanine.
- We will be doing additional asphalt paving.
- Fenced lot for vehicles.
- Drive isle for client and employee parking.
- Landscaped area.

### **PUBLIC WORKS DEPARTMENT, Jerry Nelzen**

- There is an 8 inch sewer main to the 4-lot subdivision and we believe there is a 6 inch sewer lateral stubbed to the site, but we need to confirm it and if not, you can connect to the 8 inch main on site, but it would be all private to the 8 inch main. Give me a call and we can meet on site and video scope the sewer line.
- You will work with Clackamas County for all your on-site plumbing needs.
- You cannot build over the sewer main with any type of structures. Dan said he believes the sanitary sewer is more on lot 1 rather than lot 2 and in a 12 ft utility easement, it shows it on these plans where everything is between the dotted lines. Discussion ensued. If you do not use the stub to the site, we will have to talk.
- If you utilize the 6 inch stub we want to have a 6 inch cleanout with a "Y" sweep back to the 8 inch main and located in the sidewalk. I will observe the connection and Clackamas County will do all the inspections for the private plumbing on your property.

### **CURRAN-MCLEOD ENGINEERING, Hassan Ibrahim**

- Are you planning on utilizing the existing driveway and the answer was yes. Hassan said you are not asking for any other entrances off of Township Road and the answer was no.

- SE Township Road is scheduled to be paved this summer and I do not think there is a water service to your site and we strongly suggest you get the water service across the roadway behind the sidewalk before we pave. You can coordinate with Doug Quan, Canby Utility Water Department Foreman on where and when to get the water to your site before July 1, 2016.
- You will retain all storm drainage on site by doing a drywell, planter strip, pond or however you want to contain it. If you are using Pat Sisul he is very familiar with the process. If you chose to go with a drywell, it has to be DEQ rule-authorized. Nat asked if we allow dispersal trenches similar to Portland or Lake Oswego and Hassan said yes. Nat wanted to know if we would require a civil engineering to provide the calculations at the time of submittal and Hassan said yes. I am assuming you will want a set of civil plans to accompany the architectural and the answer was yes.
- I would like you to be aware if we have paved SE Township Road and you are not ready to put the service to your lot, we have discussed it internally and it could be painful in regards to you as we may require a full frontage, half street paving. Nat asked if they could do a sleeve and Doug said it would not work because in order for that to happen we would still have to go into the roadway and reach the corp stop to turn your water on and the sleeve will not allow us to do it. Hassan said we strongly recommend you get this service in before we pave.

#### **NJP CONSTRUCTION, Nathan Parrish**

- Do you want a grading plan and the answer was yes, through the City.
- The dispersal trench for the stormwater do we send it to the city first? Hassan said we will have to do a courtesy review and Nat said before it goes to the county? Hassan said it can be simultaneously.
- Nat asked if there was any other GEO tech requirements for a commercial site here in Canby. Bryan said yes, your parking lots and foundations. Nat said from at least a city stand point all the inspector is going to want to see is a pad cert before we pour the pad, I would assume. Bryan said that is a building code issue through Clackamas County.
- Do you have your standard details on your city's website and the answer was yes. Bryan said it is called the Public Works Design Standards and Chapter 4 is stormwater and we utilize the Clean Water Services design review and Nat said we reference back to Clean Water and the answer was yes. Nat asked who would be the governing body for the project, will it be City of Canby from a storm perspective or is it going to be Clean Water and Bryan said it will not be Clean Water and it is my understanding the county has absolutely nothing to do with storm water runoff unless it is out of a pipe from the roof. Otherwise it is our responsibility to manage storm water on site. Nat said he would provide a plan for rain drains and surface drainage all go to my combined swale or rain garden assembly if that is acceptable from the city. Bryan said we need to have some analysis showing the quantity of the runoff. Hassan stated if you are using Sisul Engineering they are pretty familiar with the process and know exactly what to do, they have done hundreds of projects around Canby. Dan said he was the engineer on this 4-lot subdivision and Hassan said he could almost use the same data to size it.



**CITY OF CANBY, EROSION CONTROL, Dan Mickelsen**

- Nat asked if Dan needed an erosion control plan and Hassan said it was permitted through the city. Nat asked if we needed a construction erosion control plan and a landscape plan to service the final and Dan said yes. Nat said he assumed with this site you will not require a wheel wash, just a standard sediment fence buried in the ground for the perimeter. Dan said if you do a buffer area I may not require you to put in a sediment fence, but I need to see what you are proposing. Nat said what is the minimum buffer size you want and would you want waddle bags. Dan said you can give yourself 4 to 5 feet buffer.
- I will want to see a concrete washout area and you can put it in a base area like a parking lot. You will need to watch your subcontractors because they will wash off in areas you do not want concrete or masonry slurry.
- Just for your information there are on-site storm chambers already in use on the other lots adjacent to your site and have worked successfully. The mini-storage has some of their stormwater going into a chamber and the rest goes into infiltration swales. You have the options of having your landscaping requirement be your stormwater treatment as well. This area in Canby drains well once you are past the rock. Nat asked about parking lot drainage what does the city requires, pervious or gravel parking lots. Dan said we do not accept gravel parking lots for commercial buildings they have to be either asphalt or concrete. Nat said what about the storage area if it is just for storage and not for vehicular traffic and Bryan said that is okay. Nat said my question is if we have a gravel storage area do you require any type of drain assembly in the gravel areas or just paved areas not used for traffic. Dan said I do not know if we have anything set in stone, but I would say yes because compacted rock will only take approximately 20% of the water and the rest will be evaporation. Nat asked if a typical Lynch basin was acceptable and Dan said yes and showed what the other properties are using for their stormwater systems.
- When you apply for your erosion application it can be accessed on the city's website or pick it up at the Planning Department. If I have email addresses and good phone number contacts I can forward my comment sheets to whomever you would like. Nat asked who do we want to make as the main point of contact for communication and the answer was Bryan Brown.
- Dan asked if they were going to do irrigation to the landscaped beds and Nat said we are able to do a hose bib in lieu of irrigation with a hose stretched to the area, as I understand it from one of the city's guidelines or statutes. Doug said you plan on doing above ground sprinklers and hoses and the answer was yes. Steve said we are planning on doing native and you only have to water it for the first year and it should be self-sustaining after it.
- Dan wanted to emphasis if you are doing all natural landscape you need to make sure it gets watered. We had a company in town that did the all natural landscape and it turned out to be the driest May in the past 15 years and what did not die turned to weeds and then we had one of the wettest June's and the landscaping was not worth anything. The company had water spigots and they did not have a big enough draw like you would with irrigation and I just want you to be sure everything gets that good drink of water to get established. My thought would be put at least 4 to 5 faucets to help with the landscaping. If you do not use them after a year it came in real handy during the establishment of the landscaping.

**CANBY UTILITY, ELECTRIC DEPARTMENT, Gary Stockwell**

- We have some conduit stubbed to the south east corner of the property and that is where we will pick up the power to serve the building. You have not expressed any of your needs and single phase is readily available and obviously your cheapest option, where 3-phase could become available and Steve said we are pretty certain we are going to do the PGE 400 amp 320, do you guys offer that? Gary said we can do a single phase class 320 and would not be a problem whatsoever.
- The biggest issue I can see is we will need to set a transformer to serve you and it looks pretty tight on the site and I will need 10 ft of clear space including the eve between the transformer and the building.
- Steve questioned service size and voltage availability and Gary said we are discussing a 122/40 single phase. Steve asked is there not an adequate transformer on the pole? Gary said there are no poles anywhere near the property, it is all underground. The transformers serving the other properties are remote from your site, therefore it will need a transformer and Steve said that will be an additional costs we will have to pay? Gary said the way Canby Utility does the job is once you give me your needs, I will develop a time and materials to get power to your building. We supply vaults, pads, transformers, conduits to within 5 feet of the building and all you do is supply the trenching, staking and grades. When we complete the work and if it costs less than what you paid we will refund the remaining money. Steve said will it cost less if I went with one 200 amp panel and Gary said it will cost less if you went with a 200 amp service versus a class 320. The cost between the two service requests are a couple hundred dollars and Steve said you answered my question and it is smart money spent. Gary said I will get you a scope of the work sheet to take with you.
- Nat asked Gary what their lead time process was after we have all the fees paid and a green tag on the building and Gary said as long as you have given me your electrical needs, I can have the job drawn up and it will be just be a couple of days of actual work.

**CANBY UTILITY, WATER DEPARTMENT, Doug Quan**

- We need to get into the site before July 1<sup>st</sup>, which is when the city plans to pave SE Township road and there is no water service to this site.
- Installation costs for a main to meter service is \$4,880, the system development charge (SDC) is \$3,954 for a 5/8 x 3/4 inch meter, \$5,790 for a 1 inch service. My thinking is you will need a 5/8 x 3/4 inch meter since you are only showing a bathroom and water spigots outside. Nat asked if the \$4,800 number is that the tap fee basically and Doug said the \$4,880 is the main to meter tap fee and it includes us restoring the pavement and concrete. The meter set fee for the 5/8 x 3/4 is \$313, setup fee is \$25 and depending on what size of service you want you are looking around \$10,000. This will have to be paid so we can get them in by July 1<sup>st</sup> if you are going to move forward with this project. Nat said all fees need to be paid up front and the answer was yes. Doug said we will do the installation bringing the line to the meter box, setting the meter and you will have to go from the meter back to your building. Nat said I assume I will be in before you and can I leave a loop up close to the meter and Doug said you can if you like, but your plumber will still have to do the connect

and Nat said correct. Doug asked when do you plan on starting and Nat said as soon as they give us permits.

- Will you be operating any type of water using equipment inside the structure other than your normal bathroom stuff? The answer was no.
- Nat asked what is a reasonable lead time and Doug said a week.
- Nat asked if they cared if they used wirsbo from the meter to the building. Doug said basically you will have municipex coming into your site and it will be up to you.
- The water meter needs to be placed either on the east or west side property line.

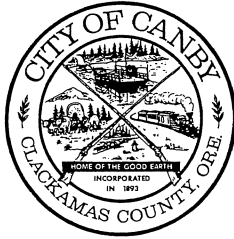
**CITY OF CANBY, PLANNING DEPARTMENT, Bryan Brown**

- Bryan handed out his memorandum for Rudnick Electric Signs, LLC. This will help you make your application submittal and I think Steve has already gotten the SDC's, but I wanted to make sure it gets in his hands. This will show all the fees for the city and just to let you know Canby Utility has their own system development charges. This memorandum gives you the details on what you need to do through the Land Use Approval and that will be towards the last part. Most of these pages are telling you what provisions of the code you have to demonstrate compliance with in your Land Use submittal and the best way to do it is by the criteria and the response. We want to make sure you have read all the review criteria and you made some assessments on whether or not it is applicable or you have met it or not.
- The most important thing I need to indicate to you is doing a traffic scoping and in order to get it going as soon as possible you need to bring a \$500 deposit to us, which will give us a notice to proceed to our traffic consultant. We will work together to develop a traffic scoping task that needs to be studied to make sure you are not having any adverse impact on the city's transportation system. They will help to determine according to our adopted standards whether it will or will not include the study of any nearby intersections. Otherwise we will be doing a traffic generation analysis of your proposed use based upon what your business is, how you are using this new building and we need to be certain of the square footage of the building to pass it along to the traffic consultants. They will come back with a scope, hopefully within two weeks and the scope will tell you if there will be any additional specific tasks to complete a study and what the study will cost. At that point you may choose another firm to complete the study or you can have DKS finish it with their estimated costs. I am guessing we will use most of the \$500 to complete the scope and the study will be around \$1,500 approximately. Nat said besides the firm you mentioned is Kittleson fairly prominent with their studies and Bryan said yes. Nat asked is there anything that needs to be done with the traffic study when the result comes to integrate into the civil plans submitted to the city or can they be completely independent. Bryan stated in an ideal world you would have the traffic study complete when you submit your application for Land Use review, the code states if we know it will be done within a week we can take your submittal. It has the potential of slowing down the scheduling of the Planning Commission meeting if we do not have everything complete when we need it.
- Looking at this conceptual site plan the parking spaces indicated on the building site here, there is about a foot or so in our designated parking dimensions of a parking space over hanging into the designated 20 ft wide access easement. My thoughts are it is not an ideal situation, but it is probably a private matter and it is between you and the other property

owners having use of the access easement. To be safe you need to discuss this with the other property owners with a guaranteed means of access and get them to agree in writing stating it will be okay to design your development with a foot or so of your parking spaces hanging over it. My other suggestion is move the building's foot print 1 foot over, but when you do that you need to be cognizant of the 6 foot wide utility easement and your eave overhang does not go into the utility easement. Steve said I can shrink the building by 1 foot or we can remove the eaves and put up gutters.

- You mentioned a multi-tenant sign and we can work with you on it, but we will have to be creative with the code to do so. Your sign would be sitting on your property and if you are going to do a multi-tenant sign I would recommend you do some type of sign easement and agreement document, which states here are the responsibilities of maintenance, if a tenant goes away or changes and how are you going to change the reader board. Also in the long term who is going to maintain the site, it protects them and you if you have some sort of legal agreement or easement. As far as the size we can work with you and since there is a residential zone across SE Township Road I think there will be a desire for you not to go much bigger than what you would otherwise qualify for as a free standing sign. I think you should keep the size in mind and it is generally an 18 ft tall pole sign and 48 sq ft and if you need something bigger we encourage you to consider a monument sign, which starts at the ground and goes up to 9 ft tall and probably 48 sq ft. We might be able to get you something bigger than that by counting the acreage of all the properties involved, but we do not want to go too much bigger because of the residential zoning.
- The only fee we have not discussed is a plan review fee of the civil plans for your pre-construction meeting and we collect it prior to the building permit. It is calculated at 0.2% of the total site construction costs excluding the building and it has a cap of \$3,000. Nat asked if the erosion control was wrapped in this fee sheet and Bryan said no, it is an application you need to turn in and it will prompt us to give you a release letter to Clackamas County for your permits. Dan said the fee is based upon the size of the site and either commercial or residential. Nat said is the grading permit intergraded into the actual building permit and Bryan said if there is a requirement from county for a grading permit they will tell you at that time.





BEFORE THE PLANNING COMMISSION  
OF THE CITY OF CANBY

A REQUEST FOR SITE AND DESIGN REVIEW)  
TO CONSTRUCT A COMMERCIAL/ )  
MANUFACTURING BUILDING AT )  
1400 SE TOWNSHIP ROAD )

FINDINGS, CONCLUSION & FINAL ORDER  
DR 16-04  
JEREMY CONRAY & RUDNICK HOLDINGS, LLC

**NATURE OF THE APPLICATION**

The Applicant has sought an approval of Site and Design Review DR 16-04 for the construction of a new 4,896 square foot commercial building at 1400 SE Township Road otherwise described as Map and Tax Lot 31E34C01817, City of Canby, Clackamas County, Oregon. The property is zoned M-1 (Light Industrial Zone) in the Canby Municipal Code (CMC) and designated Light Industrial under the Canby Comprehensive Plan.

**HEARINGS**

The Planning Commission considered application DR 16-04 after the duly noticed hearing on July 25, 2016 during which the Planning Commission by a \_\_\_\_\_ vote approved DR 16-04. These findings are entered to document the specifics of the approval.

**CRITERIA AND STANDARDS**

In judging whether or not a Site and Design Review application shall be approved, the Planning Commission determines whether criteria from the Code are met, or can be met by observance of conditions, in accordance with Chapter 16.49.040 Site & Design Review and other applicable code criteria and standards reviewed in the Staff Report dated July 14, 2016 and presented at the July 25, 2016 meeting of the Canby Planning Commission.

**FINDINGS AND REASONS**

The Staff Report was presented by staff with a recommendation for approval of the Site and Design Review application (prior to and without benefit of the public hearing) along with Conditions of Approval in order to ensure that the proposed development will meet all required *City of Canby Land Development and Planning Ordinance* approval criteria.

After holding the public hearing where written and oral testimony was received from the applicant, other proponents, those who were neutral, and opponents in attendance; the Planning Commission closed the public hearing and moved into deliberation where they utilized the findings and conditions listed in the staff report along with the overall presentation record at the public

hearing to make the following findings beyond those contained in the staff report to arrive at their decision and support their recommended conditions of approval:

### **Commission Findings and Approval for Modifications of Standards**

1. The Commission finds that

#### **CONCLUSION**

In summary, the Planning Commission adopted the findings contained in the Staff Report along with the additional findings indicated above, concluded that the Site and Design Review application meets all applicable approval criteria, and recommended that City File# DR 16-04 be approved with the Conditions of Approval stated below. The Planning Commission decision is reflected in the written Order below.

#### **ORDER**

The Planning Commission concludes that based on the record on file including testimony of the applicant and public at the public hearing, and findings of the Planning Commission that the application will meet the requirements for Site and Design Review approval. Therefore, **IT IS ORDERED BY THE PLANNING COMMISSION** of the City of Canby that **DR 16-04** is approved, subject to the following conditions of approval:

#### **CONDITIONS OF APPROVAL:**

##### **Conditions Unique to this Proposal**

1. A revised site plan shall designate the specific location planned for bike racks with approval of the final construction drawings.
2. The future planned multi-tenant pole or monument sign shall be limited to the size and height standards applicable to a "Minor Business Complex" as indicated in Table 2 of the sign ordinance in recognition of the residential uses directly across SE Township Road from the sign location and shall secure required sign permit approvals separate from this application.
3. The applicant shall provide documentation indicating the location, lumen output, and shielding aspects for any planned outdoor lighting fixtures or parking area lighting with approval of the final construction drawings.
4. The applicant shall demonstrate that a shared reciprocal ingress and access easement for his use exists along the east edge of the adjacent property for the full depth of the applicant's lot; or, 2) eliminate all encroachments indicated within the designated 20 foot wide access easement – including all landscape areas and the rear portion of the designated parking stalls as dimensioned by ordinance and increase the paved width of the access on the applicants property to a full 20 foot width. (This appears to be possible by moving the building and angle parking to the east within the designated 12 foot setback currently provided along the east boundary of the property).
5. Demonstrate on a revised site plan where 10 to 50% of the new paved parking

area shall utilize previous pavement.

### **Procedural Conditions**

#### **Prior to Issuance of building permits, the following must be completed:**

6. The design engineer shall submit to the City of Canby for review and approval at the time of final construction plan approval a storm drainage analysis and report applicable to the defined development area detailing how storm water disposal from both the building and the parking areas is being handled with calculations to justify the sizing of the proposed soakage or detention facilities. Any drainage plan analysis shall meet shall conform to the Clean Water Services storm drainage design standards as indicated in Chapter 4 of the Canby Public Works design standards dated June 2012. All on-site drywells shall be ruled authorized by the Department of Environmental Quality (DEQ). A copy of the registration shall be submitted to the City prior to any storm drainage construction.
7. A sediment and Erosion Control Permits will be required from the City prior to commencing site work. They are usually applied for with a building permit release letter when System Development Fees are due and payable.
8. Prior to the issuance of a building permit, the installation of public utilities, or any other site work other than rough site grading, construction plans must be approved and signed by the City and all other utility/service providers. A Pre-Construction Conference with sign-off on all final construction plans is required. The applicant may submit the civil construction drawings separate from the building permit submittal for final preconstruction conference sign-off approval. The design, location, and planned installation of all roadway improvements and utilities including but not limited to water, electric, sanitary sewer, natural gas, telephone, storm water, cable television, and emergency service provisions is subject to approval by the appropriate utility/service provider. The City of Canby's preconstruction process procedures shall be followed. Construction plans shall be designed and stamped by a Professional Engineer registered in the State of Oregon.
9. Clackamas County will provide structural, mechanical, grading, and review of Fire & Life Safety, Plumbing, and Electrical permits for this project. Fire & Life Safety approval must be obtained from Canby Fire District prior to issuance of a City building permit.